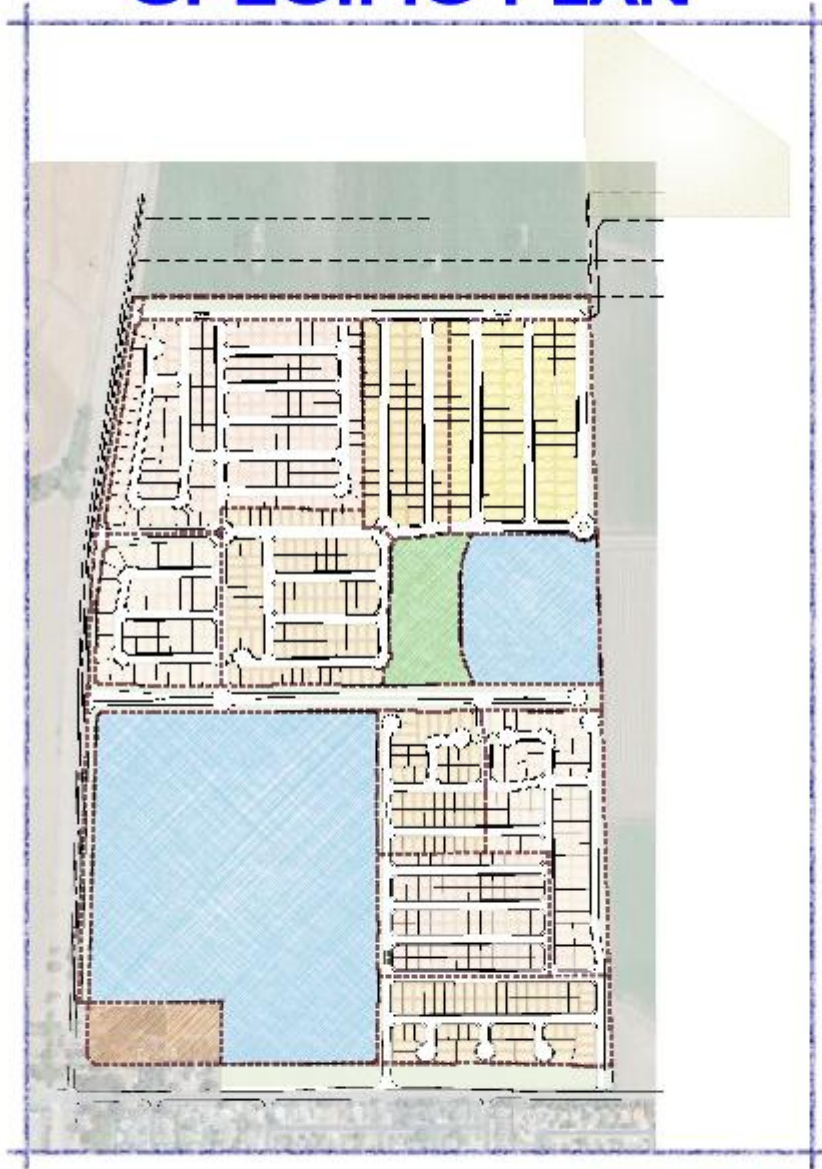


BRIGHTON LANDING SPECIFIC PLAN



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1.0 INTRODUCTION

1.1 SUMMARY

The primary objective of this Specific Plan is to create development standards and a land use plan for the Brighton Landing area that will satisfy the overall goals of the City while providing a design theme that can be expanded and utilized in projects that will eventually develop to the north and south of Brighton Landing.

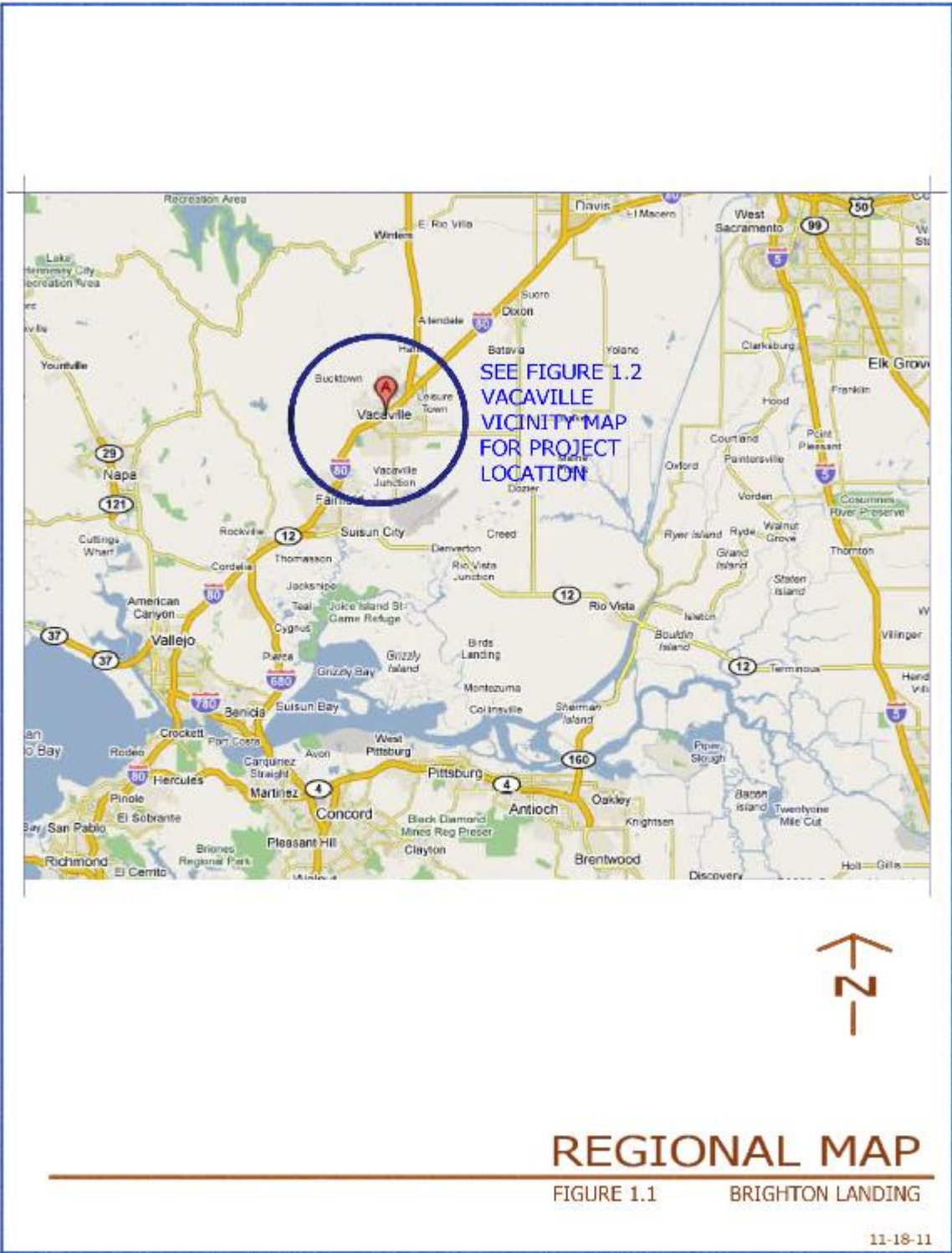
The Brighton Landing Project comprises 217.21 acres (plus 17.60 acres for the off-site detention basin for a total area of 234.81 acres) and is bounded by Leisure Town Road on the west, Elmira Road to the north, PG&E Towers to the east and the Batch Property to the south. (See Figure 1.1 Regional Map and Figure 1.2 Vicinity Map) The property consists of four (4) land owners: Brighton Landing-Vacaville LLC; John and Jetta Burnett; City of Vacaville; and Evan and Bonnie Wipf (See Figure 1.3 Ownership - Brighton Landing Specific Plan Area). There are two residential houses in the area but the majority of the area is vacant and is used mostly for dry farming (See Figure 1.4 Brighton Landing Satellite Photo).

On November 8, 2011, the City Council adopted a Preferred Land Use Alternative (PLUA) for updating the General Plan. The Brighton Landing Project is consistent with that PLUA.

The project area is proposed to include the following amenities:

- A 50 acre site set aside for construction of a regional Catholic High School.
- A new public elementary school site (comprising 11 +/- acres) that can be expanded to facilitate construction of a K-8 school if the Vacaville Unified School District chooses this option in the future by utilizing additional dedication from the Batch Property to the south.
- Improvements to Leisure Town Road (future Jepson Parkway) including provisions for widening and shifting of the road east consistent with the Jepson Parkway Plan.
- New Neighborhood Park (adjacent to the proposed new public elementary school).

- Trails throughout the development which could be connected to future development to the north and south.
- A variety of single family housing types ranging in size from 3,600 SF lots to 6,000 SF lots with an overall gross density of 3.6 +/- units/acre. Excluding the two school sites and the park yields a net density of 5.4 +/- units/acre.
- The project will provide the funds necessary for the City to secure an 80 +/- acre site adjacent to Laguna Creek for City construction of a regional detention basin.
- 6.3 acres of general commercial development on the corner of Elmira at Leisure Town (currently occupied by the 2 residential units). It should be noted that the three properties shown in this area are not part of the Brighton Landing Application and as such, three options have been included for the City to consider. The highest and best use would be to develop the property as a neighborhood commercial project. A second alternative would be to include the area in the plans for the proposed Catholic High School project. However, if the Catholic High School project is completed before the subject property becomes available; expansion of the High School would likely be problematic. The third option would be to develop the project as a small residential neighborhood. This option would allow the existing two homeowners to remain with development occurring on the remainder of the property. All three options are presented within this Specific Plan.
- Construction of a detention basin east of the PG&E towers sized to service the Brighton Landing Project and development of the Batch Property.



REGIONAL MAP

FIGURE 1.1 BRIGHTON LANDING

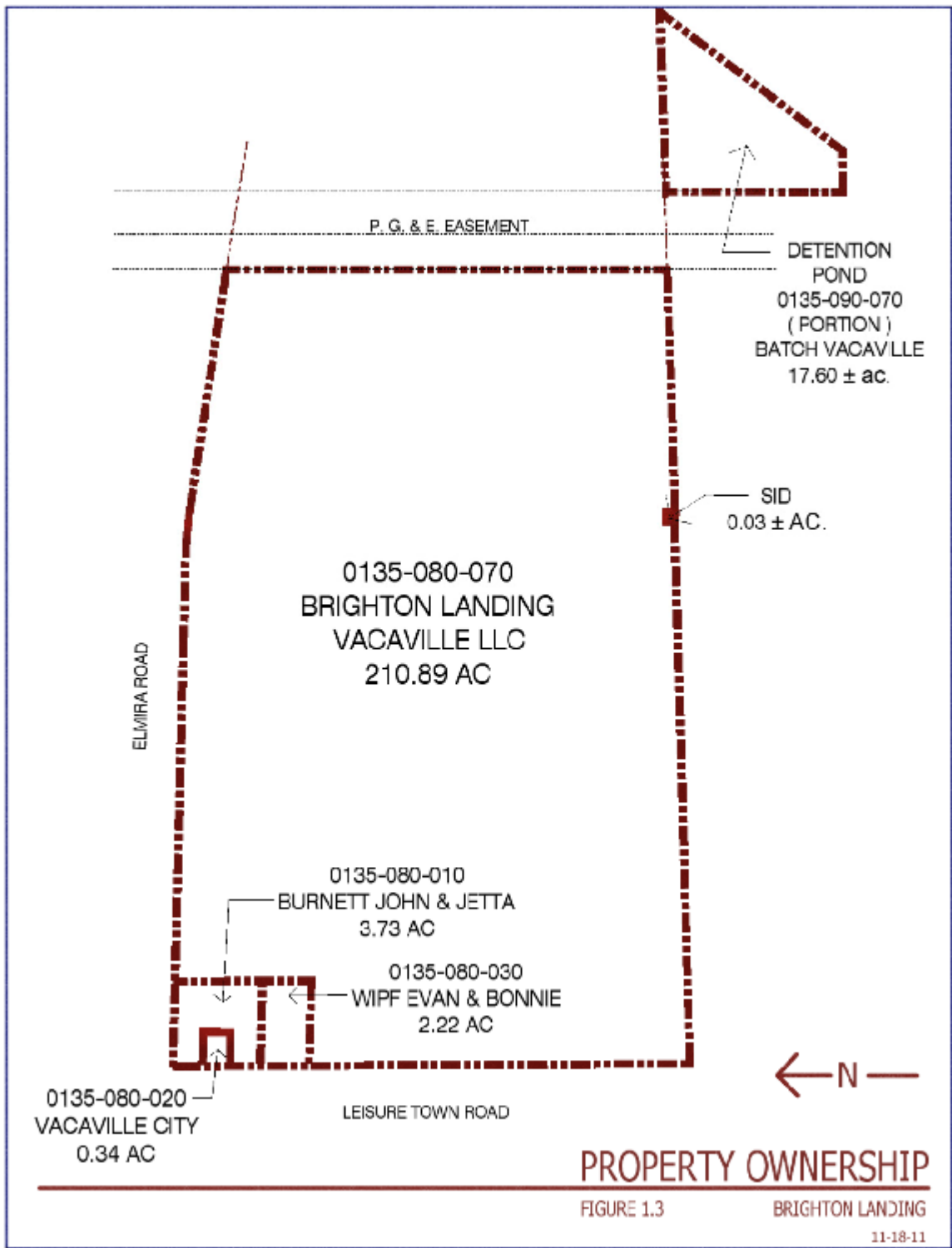
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VICINITY MAP

FIGURE 1.2 BRIGHTON LANDING

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AERIAL PHOTO

FIGURE 1.4 BRIGHTON LANDING

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The following describes the prior mentioned amenities and outlines the benefits:

1.1.1 Schools

The project consists of a 50 acre site for a private regional Catholic High School. This school is expected to generate 1,200 +/- students and attract students from the northern Solano County region and parts of Yolo County. This school will bring parents and students to the City who will shop and dine in Vacaville, increasing sales tax revenues.

In addition to the private high school, the project area includes 11.0 acres for the construction of a Public Elementary School for the Vacaville Unified School District. At the time of preparation of this Specific Plan, the School District had not rendered a decision on their specific needs for a school in the area. As a minimum, they envision the need for an elementary school but the District is currently working on a policy that would require all new schools to be configured to serve as a K-8 facility. The 11 acre site shown in this Specific Plan would serve both options: it is adequately sized to provide for an elementary school or it could be ½ of a future K-8 school. The PLUA adopted by the City Council includes development potential on the Batch property to the south of Brighton Landing and the potential for the expanded school site. Therefore, if a K-8 school is required by the School District rather than the elementary school provided in this application, the additional land would be dedicated with the future Batch development.

1.1.2 Improvements to Leisure Town Road (Jepson Parkway)

Recently the Solano Transportation Authority (STA) adopted the Jepson Parkway Plan Line a portion of which comprises Leisure Town Road. With development of the Brighton Landing area frontage roadway and landscaping improvements will be funded for future construction along this key corridor.

1.1.3 New Neighborhood Park

Development of the Brighton Landing area will result in the construction of a new 6.0 acre Neighborhood Park adjacent to the proposed public school site. In the past, for parks adjacent to schools, the Vacaville Unified School

District and the City of Vacaville have had joint use agreements regarding park usage which increase the park's usability and functionality. With the configuration of the park as proposed, the park could be maintained as an entity separate and distinct from the school. If the city and the school were to decide that a joint use agreement was in their mutual interest, the park could be jointly used by the city and the district. The layout proposes that the trail shown would be on property dedicated to and maintained by the City. The School District could decide to place a fence along the path to separate the park from the school or leave it open for ease of access. The situation between the District and the City is fluid and this section may need to be revised depending on future agreements.

1.1.4 Trail System

In addition to the new park, a series of trails will be constructed throughout the development which will provide a pedestrian friendly connection between the park, the two schools and a trail system that could be extended north and south consistent with the adopted PLUA. The Brighton Landing Project will be constructing approximately 3.11 miles of trails and when combined with the park and landscaping areas comprise 21.07 acres of dedicated open space and recreational area.

1.1.6 Variety of Housing Types

The Brighton Landing development proposes a variety of single family detached housing types with an emphasis on moderate density development. The City of Vacaville General Plan policies indicate that development in outlying areas construct a minimum of 25% moderate density housing (lots 4,000 SF and less) to increase the City's existing moderate density inventory which is currently at 20% +/- . The Brighton Landing development proposes a total of 64.8 % +/- moderate density small lot single family product. The remaining 35.2% of the project will be standard low density product.

1.2 LEGAL CONTEXT

1.2.1 Authority to Prepare

A Specific Plan is a planning and regulatory tool made available to local governments by the State of California. By law, Specific Plans are intended

to implement a city or county's General Plan through the development of policies, programs and regulations that provide an intermediate level of detail between the General Plan and individual development projects. As a mechanism for the implementation of the goals and policies of a City's General Plan, State law stipulates that Specific Plans can only be adopted or amended if they are consistent with the jurisdiction's adopted General Plan.

The authority to prepare and adopt Specific Plans and the requirements for its content are set forth in the California Government Code 65450 through 65457. The law requires that a Specific Plan include text and diagrams that specify:

- The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
- The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.
- Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
- A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out the items listed above.

1.2.2 Relationship to the General Plan

As mentioned previously, the City recently adopted the PLUA for updating the General Plan and the Brighton Landing Specific Plan reflects that adopted PLUA. The Brighton Landing Specific Plan and the City's Updated General Plan provide a framework to guide future land use and development decisions within the 234.81 acre planning area. The Specific Plan is consistent with, and functions as an implementation tool of the Updated General Plan in three ways:

- A) By acting as a statement of planning policy that refines the general plan policies applicable to a specific plan area;
- B) By directly regulating land use and;
- C) By bringing together detailed policies and regulations into a focused development program.

The City's Current General Plan Land Use Element, as updated, has specific implementing policies for the Brighton Landing area. The following General Plan Guiding Policies are addressed in the Brighton Landing project and the Brighton Landing Specific Plan as follows:

2.1-G1 Maintain Vacaville as a free-standing community surrounded by foothills, farmland and other open space.

The project complies with this policy.

2.1-G3 Establish open space linkages by preserving habitat areas, including natural creek corridors. Use utility easements where possible as open space linkages.

With 3+ miles of trails this project complies with this policy.

2.1-G4 Minimize conflicts between agriculture and urban uses and provide for a transitional area or buffer between agriculture and urban uses.

The Brighton Landing Project provides a 500' agricultural buffer with public usage of the buffer limited to a 185' linear trail.

2.1-G5 Design aesthetically pleasing roadways, including a loop street system lined with trees or other appropriate landscaping, that connect Vacaville neighborhoods and serve planned development. Streets alone should not be used to set the outer limits of urbanization.

The Brighton Landing Project complies with this policy in that the eastern boundary of the project is designed to have a linear trail and significant landscaping. In addition, the streets and trails

within the development are all interconnected to provide excellent access for vehicles, bikes and pedestrians alike.

2.1-G8 Preserve the predominant single-family residential character of Vacaville while providing other housing opportunities. Protect established neighborhoods from incompatible uses.

This project complies with this policy by providing for the largely unmet housing need of low-medium density housing.

2.1-G9 Preserve scenic features and the feel of a city surrounded by open space, and preserve view corridors to the hills, and other significant natural areas.

This policy is complied with by the establishment of the buffer and the linear park with significant landscaping.

2.1-G10 Protect the natural environment that the City enjoys and use creeks, hills, utility corridors, viable agricultural lands or other significant natural features wherever appropriate to establish ultimate City boundaries.

This project proposes to mitigate for the loss of farmland by preserving agricultural lands, on an acre for acre basis, on the east side of the PG&E towers. The preservation of agricultural lands through the purchasing of development rights will ultimately provide a significant agricultural boundary around the City of Vacaville.

In addition to the above Guiding Policies there are a number of Guiding Policies related to future growth. A key policy relates to the City of Vacaville/Solano Irrigation District Master Water Agreement. This Specific Plan recognizes that the Master Water Agreement will need to be amended to allow the Brighton Landing Project to proceed. It is understood that discussions are ongoing between the City and SID and the Brighton Landing Project may need to be revised to comply with the ultimately amended agreement.

The Updated General Plan (anticipated adoption in 2012) will likely require that projects east of Leisure Town Road facilitate the development of a range of housing densities and opportunities, pedestrian and bicycle friendly design, neighborhood commercial sites, and recreational and neighborhood facilities, by including the following requirements:

- *A network of landscaped pedestrian/bike corridors that connect key elements of the area: such as the schools, the Park and arterial streets.*

This is provided for in the Brighton Landing Specific Plan.

- *New development adjacent to existing homes within the City limits shall match or exceed the size, character, and quality of adjacent homes and lots.*

This is provided for in the Brighton Landing Specific Plan.

- *All new residential development shall conform to the Residential Design Requirements for New Single Family Development.*

This is provided for in the Brighton Landing Specific Plan.

- *Land shall be reserved for community uses such as: private schools, membership organization, day care centers, and senior centers.*

Land has been reserved for a public school, a large regional private high school, a large neighborhood park and numerous trails.

- *A financing mechanism for all public facility improvements shall be established before development occurs.*

All infrastructure improvements will be paid for by developers of the Brighton Landing project. A Brighton Landing Benefit District will be established for this area to ensure that the developer of each portion of the project will pay for its fair share of public facility improvements. Included within the Benefit District will be provisions related to phasing and timing of various improvements. In addition, provisions will be made to make

sure infrastructure is sized to accommodate development to the north and south of Brighton Landing as outlined in the adopted PLUA. Other funding mechanisms that may be explored include the possible establishment of an Assessment District, Mello-Roos District, or a Community Facilities District. The chosen funding mechanism will be established with approval of the initial large tentative map.

- *Elmira Road shall be widened to the City standard width through the project sites for all projects that front on these streets.*

This street widening adjacent to the proposed project component will be constructed with the Brighton Landing development.

- *Leisure Town Road shall be widened and improved to the standards for the Jepson Parkway along the frontage of all projects that abut to Leisure Town Road.*

Land for the ultimate widening of Leisure Town Road will be dedicated with the initial large lot Final Map. This dedication includes an additional 24 feet beyond that specified in the Jepson Parkway Plan to allow for widening of Jepson Parkway to 6 lanes if it is ever required in the future. Funding for the improvements will be provided, in part, by the following entities: The Solano Transportation Authority; the City of Vacaville, and by the developers of Brighton Landing. The exact responsibilities will be worked out in subsequent agreements associated with approval of the various tentative maps.

- *On September 21, 2004, the Vacaville City Council enacted a policy whereby Leisure Town Road would be limited to 4 lanes. Further, to lessen impact on those existing residences on the west side of Leisure Town Road the road would be shifted 35' to the east. As part of that policy, it was determined that a "Limited Access Collector Road", paralleling Leisure Town Road be constructed to facilitate traffic movement from the southeast part of Vacaville to the freeway to the north. All project development east of Leisure*

Town Road (Jepson Parkway) will be required to construct the “Limited Access Collector” in an alignment approved by the Public Works Director.

This “Limited Access Collector” is part of the Brighton Landing project and will be constructed with development.

- *Different development projects east of Leisure Town Road shall coordinate their respective roads, bike paths, landscape corridors and design standards to create a unified sense of place and identity.*

Roads, trails, landscape corridors are coordinated within the Brighton Landing Project and will set the design theme for development east of Leisure Town Road which will provide continuity between the projects.

- *Commercial buildings shall be no more than an average of 30 feet in height, and be designed to front on the sidewalk, with parking at the rear of the property, when feasible, so as to enhance neighborhood aesthetics and to encourage pedestrian – friendly design.*

Commercial buildings will be limited to 30 feet and will have buildings oriented to the street with parking in the rear.

- *Infrastructure master plans for sewer, water, storm drain, and traffic improvements shall be prepared prior to or in conjunction with the processing of subdivision maps for all development within the Brighton Landing area.*

Master plans for sewer, water, storm drainage and traffic improvements were created for the Brighton Landing project.

- *Prior to the approval of any subdivision applications, the developers shall assure that all required domestic water supply and distribution systems, wastewater collection and treatment facilities, storm water management facilities, and roadway segment and intersection improvements will be incorporated into the final project plans.*

The Brighton Landing Project will incorporate the infrastructure as outlined in the master plans mentioned previously.

1.2.3 Purpose and Intent

The Specific Plan provides both general guidelines and specific standards for the development of Brighton Landing and sets the parameters for a variety of land uses; thus the purpose of the Specific Plan is to provide a set of plans, policies, regulation and implementation programs for guiding and ensuring orderly, future development in accordance with the adopted Land Use Plan within the General Plan.

1.2.4 Environmental Review

The adoption of a Specific Plan is a “project” under the California Environmental Quality Act (CEQA) and the CEQA Guidelines. To meet CEQA requirements, an Environmental Impact Report (EIR) will be prepared as part of the process in adopting this Specific Plan, the Rezoning application and the Tentative Map application. As mentioned previously, the Specific Plan is consistent with the recently adopted PLUA. An EIR will be prepared which analyzes the Brighton Landing Project and this Specific Plan. During this process, it is anticipated that the Developer and the City of Vacaville will negotiate a Development Agreement for the project.

Also, the EIR will identify any specific mitigation measures which will need to be complied with as the project moves forward towards construction. Needed mitigation measures will be included in the Conditions of Approval for the Tentative Map and implementation will be required within the final Subdivision Improvement Agreement which is executed between the developer and the City prior to construction. The Subdivision Improvement Agreement requires the posting of surety bonds and payment of fees to insure compliance with the EIR, DA, and project Conditions of Approval.

1.2.5 Development Agreement

It is anticipated that, as part of the project approval process, a Development Agreement (DA) will be entered into between project proponents and the City of Vacaville. Typically a Development Agreement within the City of Vacaville focuses on the following: Community Benefit

Contribution (CBC); Community Facilities District #12 (CFD12), timing of Park Construction, timing of infrastructure improvements, and specific project “deal points”.

With regard to the CBC and CFD12 the developer is in agreement with these fees. With respect to the timing of the Park Construction, a Park Fee Agreement will be negotiated which will establish the timing for construction of the Park. Typically, the developer, in lieu of paying park impact fees, would expend certain funds towards the park as development of the project begins (for example land dedication and preparation of the improvement plans). Once a certain number of homes are constructed (to be negotiated) the developer would then begin construction of the park (which may be phased, again subject to negotiation and beyond the scope of this Specific Plan).

Additional “deal points” related to the Brighton Landing project involve the dedication of approximately 80 acres of land in Lagoon Valley (adjacent to Laguna Creek) for construction of a city detention basin benefiting the entire City and sale of a portion of the property (anticipated to be 50 acres) to the Sacramento Diocese for construction of a regional private high school.

1.3 PLANNING CONTEXT

1.3.1 Regional Context

The Specific Plan area is located in northern Solano County, in the southeastern corner of the City of Vacaville, as shown in the Vicinity Map in Figure 1.2. Vacaville is at the midpoint between San Francisco and Sacramento along Interstate 80, the major transportation link between these two growing urban areas as shown on the Regional Map Figure 1.1.

1.3.2 Site Context

1.3.2.1 Project Area Description

The Brighton Landing Project comprises 234.81 acres (including the detention basin area on the Batch property) and is bounded by Leisure Town Road on the west, Elmira Road to the north, PG&E Towers to the east and the Batch Property to the south. (See Figure 1.1 Regional Map and

Figure 1.2 Vicinity Map.) The detention basin to be located on the Batch Property consists of 17.60 acres.

1.3.2.2 Property Ownership

Property Owners within the Brighton Landing Specific Plan Area (with Assessors' Parcel Numbers (APN) and property acreage) are listed in Table 1.1.

Table 1.1 Parcels and Ownership

APN	Owner	Acres
0135-080-010	John & Jetta Burnett	3.73
0135-080-020	City of Vacaville	0.34
0135-080-030	Evan & Bonnie Wipf	2.22
0135-080-070	Brighton Landing – Vacaville LLC	210.89
No APN	Solano Irrigation District	0.03
0135-090-070	Batch Development Corporation	17.60
Total Area		234.81

See Figure 1.3 for a graphical depiction of the parcels noted above.

1.3.2.3 Detention Basin Beyond Urban Growth Boundary

The proposed detention basin, to be located on the Batch property is located east of the Urban Growth Boundary which is beyond the future City limits. The detention basin does represent development and is merely a public facility constructed to ensure that downstream flooding does not occur as a result of the proposed Brighton Landing project. It should be noted that the basin has been sized to ensure that post development runoff is less than predevelopment runoff. Also, additional capacity has been added to the basin to significantly reduce existing flooding in the area. In addition, the basin will serve as a water quality feature by allowing particulate matter to settle out prior to being discharged into the downstream water course.

It should be noted that the establishment of detention basins outside the City limits is common. Four detention basins are currently planned for or exist in the Pleasants Valley area west of the City Limits.

2.0 VISION AND GOALS

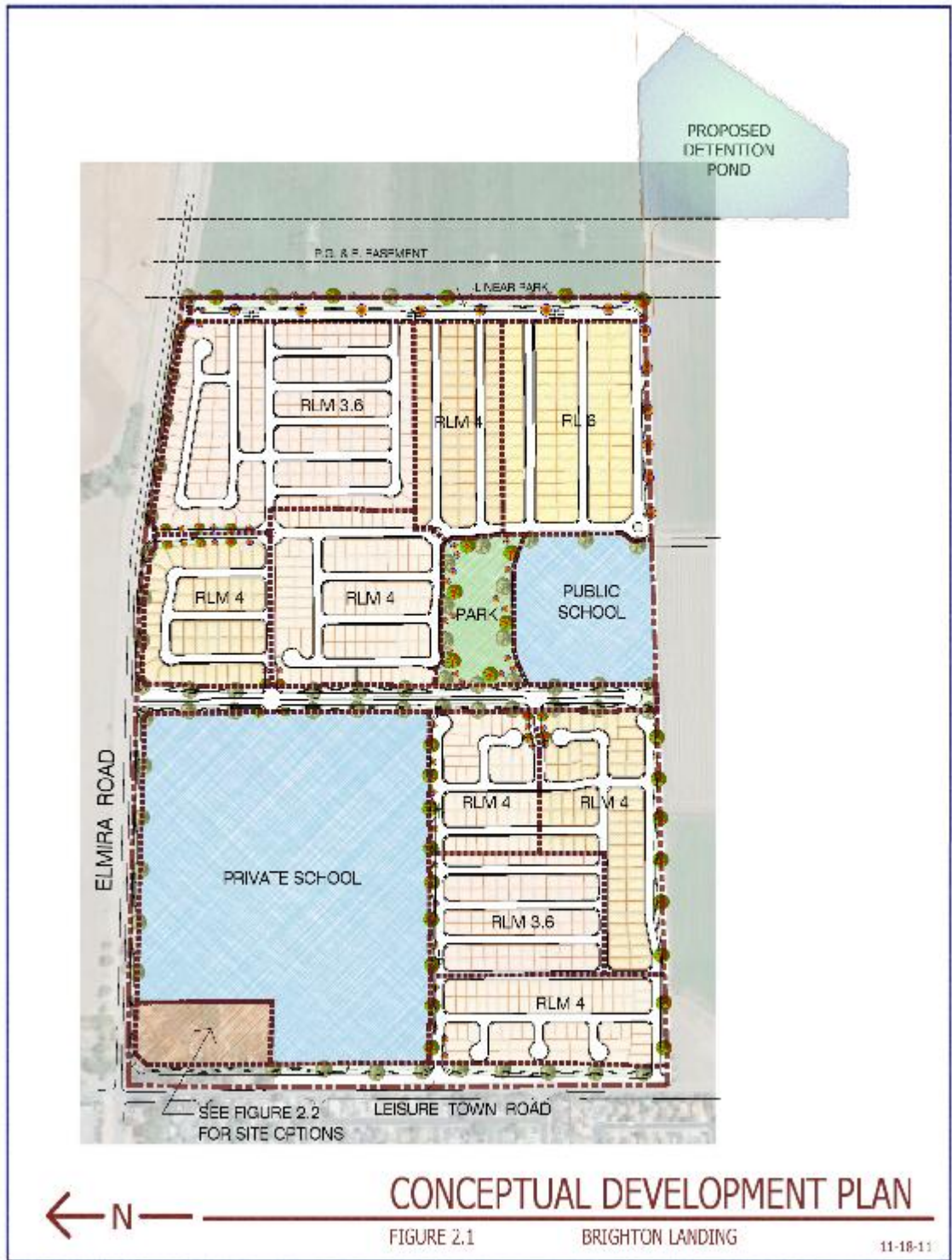
2.1 PROJECT VISION

The Brighton Landing Project proposes to set the stage for theme and character for residential projects to be developed east of Leisure Town Road. Brighton Landing will prominently feature trails and landscaping corridors which connect the large regional private high school, the neighborhood park, the public school, and transit facilities. (See Figure 2.1 for the Brighton Landing - Conceptual Development Plan.)

Brighton Landing would also provide for either an elementary school or half of a K-8 school. This school would be adjacent to a proposed City neighborhood park, creating a major focal point in the Brighton Landing area.

Brighton Landing provides for a diverse residential project with densities ranging from low density to moderate density with individual unique neighborhoods interconnected into a sustainable planned community. The design details for each of the unique neighborhoods will be developed during the design review process. Elements that could be included in the establishment of neighborhood identities are: architectural themes; street trees; colors; setback variations; and trail amenities.

Policies and statements in the City of Vacaville General Plan indicate a strong desire to have more moderate density housing (Lots 4,000 SF or less) in the outlying areas. To that end, Brighton Landing proposes that 64.8% of the residential units will be moderate density housing. This moderate density housing would comprise a variety of low medium density small lot single family residential. These residential products fit within the moderate density category as specified in the General Plan.





2.1.1 Significant Public Benefits

There are numerous Public Benefits that will occur with the development of the Brighton Landing area. These benefits are as follows:

2.1.1.1 Schools

Private High School - The Catholic Diocese in Sacramento is under contract to purchase the land for, and construct a 50 acre regional private high school. Design details for the private high school will be provided during the design review process. In designing the site, the developers of the school are encouraged to provide connectivity to the surrounding trails and ensure that the design of the buildings are compatible with the surrounding residential architectural. Exterior Landscaping should be compatible with the landscaping used in the trail system to provide continuity. Fencing on Leisure Town Road would be a masonry sound wall because of high traffic volumes anticipated. The High School is encouraged have its main entrance on Elmira Road with the main parking lot facing Elmira. If fencing is required along Elmira it is encouraged to be an open style fence unless there are buildings in relative proximity to Elmira Road which will require some sound attenuation. Fencing along the major collector and the southern property line are encouraged to be open style fencing. Pedestrian access points are encouraged for all sides to enable students that live within the Brighton Landing Project to walk to school.

Public Elementary School - In addition, land is set aside for the construction of an 11 acre public elementary school. Recent discussions with the school district indicate that the Vacaville Unified School District may require that a K-8 or a middle school would be built on the site instead of the elementary school. If the District decides that either of the other alternatives is preferred over the elementary school, the land set aside in the Brighton Landing Project will comprise the northern half of the selected alternative and the southern half would be obtained with development of the Batch Property. Provisions for these two alternatives are shown in the PLUA.

2.1.1.2 Improvements to Leisure Town Road (Jepson Parkway)

Recently the Solano Transportation Authority (STA) adopted the Jepson Parkway Plan Line which comprises Leisure Town Road and Vanden Road

South of Leisure Town Road. With development of the Brighton Landing area frontage roadway and landscaping improvements will be planned for and funding put in place for construction along this key corridor serving Fairfield and Vacaville when it is determined to be necessary and feasible.

2.1.1.4 Neighborhood Park

Development of the Brighton Landing area will result in the construction of a new 6+ acre park adjacent to the proposed school site. This park size exceeds the City standard for neighborhood parks in that 1.8 acres of neighborhood park per 1,000 residents is required which would result in only 4.56 acres of park instead of the proposed 6 acres. The Vacaville Unified School District and the City of Vacaville should be encouraged to enter into a joint use agreement regarding this park to increase its usability and functionality.

2.1.1.5 Trails

In addition to the new park, a series of trails will be constructed throughout the development which will provide a pedestrian friendly connection with the park, the schools and with the trail system envisioned for east of Leisure Town Road. The Brighton Landing project will be constructing approximately 3.11 miles of trails. These trails along with the park and landscaping comprise 21.07 acres of dedicated open space and recreational area.

2.1.1.6 Variety of Housing Types

The Brighton Landing development proposes a variety of housing types with an emphasis on moderate density development. The City of Vacaville General Plan policies indicate that development in outlying areas construct a minimum of 25% moderate density housing to increase the City's existing moderate density which is currently at 20% +/- . The Brighton Landing development proposes a total of 64.8 % +/- moderate density (lots that at 4,000 SF or less).

2.1.2 Recreation

With the construction of the park and trail system, Brighton Landing will provide unique opportunities for walking, running and biking. Play areas

will be much larger than traditional parks because the park area will be combined with the public school play areas to provide an expansive recreational area.

2.1.3 Housing

As mentioned previously, the vision of the Brighton Landing development is to provide a variety of housing with a stronger emphasis on moderate housing.

2.1.4 Superior Quality

The Brighton Landing Project vision is to provide a superior quality community focused on schools, parks, and trails tying into the superior quality of Vacaville development.

2.1.5 Connectivity

The Brighton Landing layout will provide excellent pedestrian and biking connectivity to the Regional Catholic High School, the proposed elementary school and the Brighton Landing Park. In addition, with ultimate development east of Leisure Town Road as outlined in the 2012 General Plan, the major north-south collector road will be completed which will improve north-south travel from the southeast portion of Vacaville to Interstate 80 to the north while providing relief to projected traffic congestion on Leisure Town Road.

2.1.6 Civic and Public Benefits

The civic and public benefits have been enumerated in several other locations within this Specific Plan. Succinctly, the civic and public benefits include the regional private high school, the public elementary school and park, improvements to Leisure Town Road (Jepson Parkway), and numerous trails.

2.1.7 History

There is little in the way of development history regarding this area. However, preparations for development of the Brighton Landing area have been in the making since 2003, which is when the property owners began

discussions with the City of Vacaville regarding potential development. The property has been within the City of Vacaville for a number of years.

The PLUA for the Updated General Plan has designated this area for development. Because this property is one of only two properties east of Leisure Town Road within the City Limits it is destined to be slated for development first when considering projects east of Leisure Town Road.

2.1.8 Environment and Sustainability

The Brighton Landing Development proposes a park and trail system which will enhance recreational opportunities for the residents of the area while also enhancing the open space feel and visual amenities.

2.2 LAND USE CONCEPTS

The land use concept for the 234.81 acre Brighton Landing development is a mix of residential uses complemented by the proximity to school(s), park and trails. The Brighton Landing Specific Plan includes the following Land Use Goals:

- 1. Goal: Provide for a quality project by use of high standards for design of the homes, landscaping, streetscape, and public amenities.***
- 2. Goal: Include a variety of housing types, including an emphasis on moderate density housing (particularly small lot single family residential) that is in compliance with the goals and policies of the Vacaville General Plan. The overall density has been established at 4.2+/- units/ acre.***
- 3. Goal: Provide a comprehensive system of trails, park and back up landscaping which will comprise a total of 21.07 +/- acres.***

2.3 HOUSING GOALS

Brighton Landing has been designed to have 12 distinct villages (each a separate Phase of construction). Each village will have a separate architectural theme. It is envisioned that phases will be sold to different

builders who will present their vision for their particular village. The design theme will be subject to review and approval of City staff and Planning Commission.

Goal: Provide a high variety of house plan types and exterior configurations with varied options and styles which will create distinct neighborhoods.

2.4 OPEN SPACE GOALS

Goal: Plan a system of street and trail connections that link key elements (school(s), park, and trails) and which provides a template for a future trail system east of Leisure Town Road.

2.5 OTHER GOALS

The Brighton Landing Specific Plan goals for land use and design, circulation, resource management, public utilities and services can be found in their respective chapters:

Chapter 3 – Land Use

Chapter 4 – Transportation and Circulation

Chapter 5 – Recreation, Open Space and Resource Management

Chapter 6 – Community Design

Chapter 7 – Public Facilities

Chapter 8 – Community Services and Facilities

3.0 LAND USE

3.1 INTRODUCTION

The Brighton Landing Specific Plan establishes a comprehensively planned community that is designed to set the stage for development east of Leisure Town Road. The Land Use component of the Specific Plan is primarily residential with the non-residential uses comprising the school(s) and park.

3.2 LAND USE GOAL

Goal: To establish an attractive community that provides a variety of residential and recreational opportunities.

3.3 THE DEVELOPMENT PLAN

Brighton Landing is a planned community that is designed to integrate residential opportunities, with school(s), park, and trails.

3.4 PLANNING SUBAREAS

The Brighton Landing area is divided into 17 Subareas (See Figures 3.1 and 3.2). These subareas have various land use designation. Table 3.1 details each of the subareas.

Subareas A – N are owned by the Hearthstone and comprise 210.89 acres of the total Specific Plan area. In addition to the 210.89 acres there is a small portion of land 0.03 acres that is owned by S.I.D. which will become part of the project area when that land is abandoned by S.I.D. (S.I.D. abandons their right of way when the area is no longer used for irrigation purposes.) Subarea O is owned by three different individuals as previously detailed in Figure 1.3.

Subarea A is a 50.14 acre site designated as Community Facility and is the location of the regional private Catholic High School.

Subarea B is an 11.27 acre site designated as Residential Low Medium Density with a zoning of RLM 4.0 and is intended to be developed as single family residential with a minimum square footage for the lots of 4,000 SF.

Subarea C is an 11.38 acre site designated as Residential Low Medium Density with a zoning of RLM 3.6 and is intended to be developed as single family residential with a minimum square footage for the lots of 3,600 SF.

Subarea D is a 12.95 acre site designated as Residential Low Medium Density with a zoning of RLM 4.4 and is intended to be developed as single family residential with a minimum square footage of 4,400 SF.

Subarea E is an 8.28 acre site designated as Residential Low Medium Density with a zoning of RLM 4.0 and is intended to be developed as single family residential with a minimum square footage for the lots of 4,000 SF.

Subarea F is a 10.14 acre site designated as Residential Low Medium Density with a zoning of RLM 4.4 and is intended to be developed as single family residential with a minimum square footage of 4,400 SF.

Subarea G is a 15.79 acre site designated as Residential Low Medium Density with a zoning of RLM 4.0 and is intended to be developed as single family residential with a minimum square footage of 4,000 SF.

Subarea H is a 26.20 acre site designated as Residential Low Medium Density with a zoning of RLM 3.6 and is intended to be developed as single family residential with a minimum square footage for the lots of 3,600 SF.

Subarea I is a 10.11 acre site designated as Residential Low Medium Density with a zoning of RLM 4.8 and is intended to be developed as single family residential with a minimum square footage of 4,800 SF.

Subarea J is a 16.72 acre site designated as Residential Low Density with a zoning of RL-6 and is intended to be developed as single family residential with a minimum square footage of 6,000 SF.

Subarea K totals 6.0 acres and is designated Community Facility which has been designated for the Public Park.

Subarea L totals 11.05 acres and is designated Community Facility which has been designated for the Public School.

Subarea M totals 5.76 acres and is designated Community Facility which includes the street and agriculture buffer area along the eastern perimeter of the site.

Subarea N totals 15.14 acres and is designated Community Facility which includes the following streets and adjacent trails: Leisure Town Road; Elmira Road and the Major Collector.

Subarea O is a 4.8 acre site designated for three possible options for development. Option 1 would allow for Neighborhood Commercial development conceptually as shown on Figure 2.2. Option 2 would be for the property to be incorporated into the development of the private high school. Option 3 would allow the property to be developed as Residential Low Density is also conceptually shown on Figure 2.2.

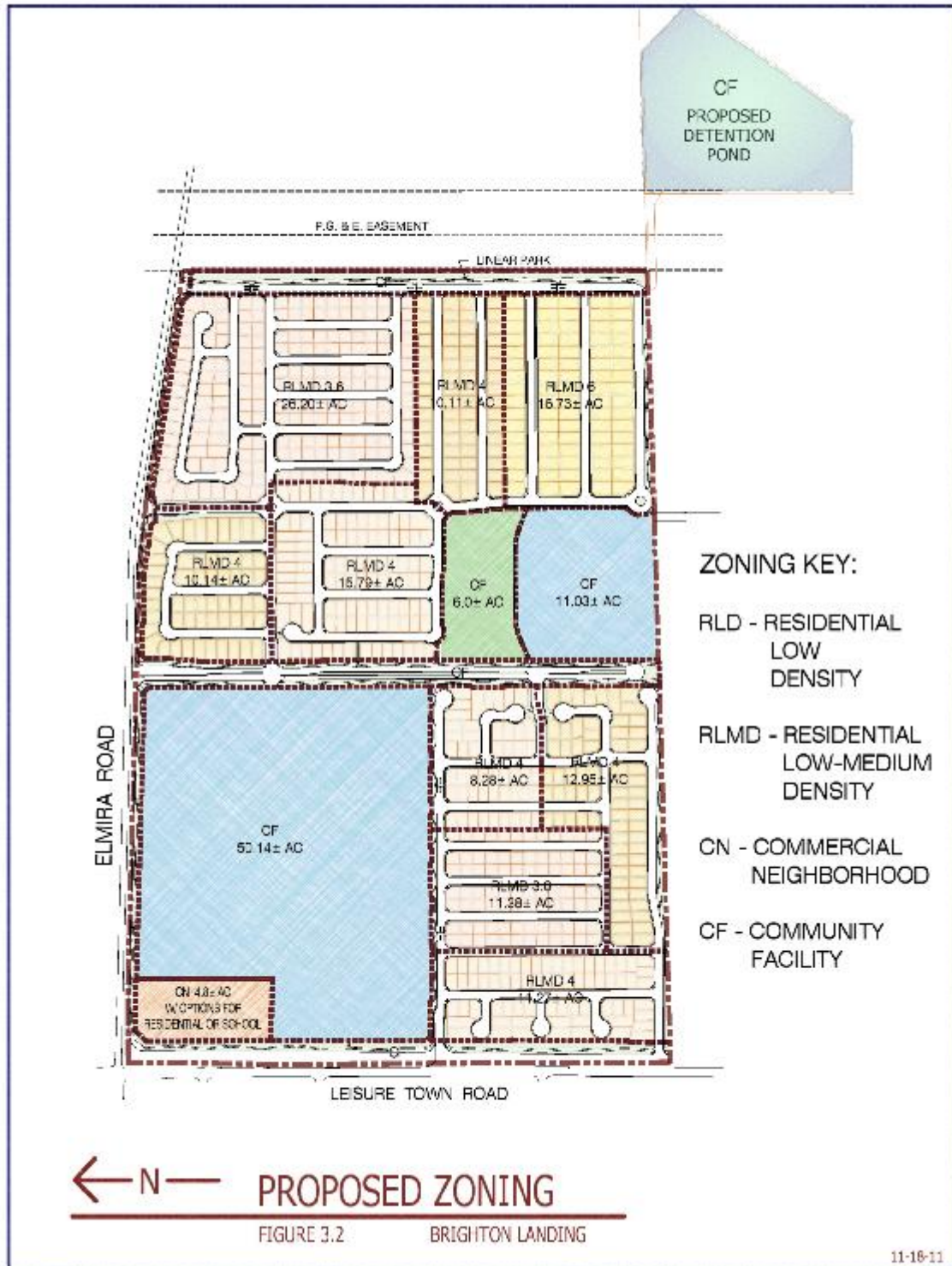
TABLE 3.1 – BRIGHTON LANDING LAND USE SUMMARY
(Refer to Figures 3.1 and 3.2 for Location of Areas)

Sub Area	Land Use	Proposed Zoning	Approximate Area (Acres)	Residential Units	Density (Units/Acre)	Non Residential Uses
A	CF	CF	50.14	N/A	N/A	Private HS
B	RLMD	RLM 4.0	11.27	68	6.0	
C	RLMD	RLM 4.5	11.38	84	7.4	
D	RLMD	RLM 4.4	12.95	70	5.4	
E	RLMD	RLM 4.0	8.28	50	6.0	
F	RLMD	RLM 4.4	10.14	57	5.6	
G	RLMD	RLM 4.0	15.79	106	6.7	
H	RLMD	RLM 3.6	26.20	190	7.2	
I	RLMD	RLM 4.8	10.11	64	6.3	
J	RLD	RL 6	16.73	80	4.8	
K	CF	CF	6.00	N/A	N/A	Public Park
L	CF	CF	11.03	N/A	N/A	Public School
M	CF	CF	5.76	N/A	N/A	Road & Trail East Boundary
N	CF	CF	15.14	N/A	N/A	Public Streets
O	CN	CN	4.78	N/A	N/A	Neighborhood Commercial (w/option for residential)
O1*	RLD	RL 8	4.78	13	2.72	Residential Option for Corner
P	CF	CF	1.51	N/A	N/A	Street Dedication for Corner
Q	CF	CF	17.60	N/A	N/A	Detention Basin
TOTAL			234.81	769 (782*)	3.54**	

* - The "O" Subarea has the option of going with Residential Low Density which is shown as O1

** - 3.54 Units/Acre represents the Gross Residential Density excluding the off-site detention basin. The Net Residential Density Excluding the Basin, the School Properties (11.03 Ac), the Park (6.00 Ac), and the Neighborhood Commercial (4.78 Ac) is **3.94 units per acre**. If the residential option (O1) is chosen for the 4.78 acre area parcel at the corner of Leisure Town Road and Elmira, the Net Residential Density is **3.91 units per acre**.





3.5 RESIDENTIAL ELEMENT GOALS

As mentioned previously, Brighton Landing has been designed to have 12 distinct villages (each a separate Phase) which will each have separate architectural themes. It is envisioned that phases will be sold to different builders who will present their vision for their particular village. The design theme will be subject to review and approval of City staff and Planning Commission.

1. Goal: Create a unique residential environment.

2. Goal: Provide a high variety of house plan types and exterior configurations with varied options and styles which will create distinct neighborhoods.

3. Goal: Encourage a range of housing types with an emphasis on single family moderate density development while addressing the policies and goals in the City of Vacaville General Plan.

4. Goal: Ensure that each neighborhood has direct access to the interconnected trail system.

5. Goal: Create houses that minimize energy use by utilizing the “Build it Green” concept in constructing the homes.

The policies in the Vacaville General Plan discuss the desire to increase the amount of moderate density housing within the City. Specifically those policies require that new outlying areas of the City have a minimum of 25% moderate density residential. Moderate Density, per the General plan, comprises the Land Uses of: Residential Low-Medium Density and Residential Medium Density. The proposed residential breakdown for the Brighton Landing development is listed in Table 3.2.

TABLE 3.2 – BRIGHTON LANDING RESIDENTIAL LAND USE SUMMARY

	Land Use	Approximate Area (Acres)	Residential Units	Percentages	Total Percentages
LOW DENSITY	RE	0	0	0%	10.40%
	RLD	16.73	80	10.40%	
MODERATE DENSITY	RLMD	106.12	689	89.60%	89.60%
	RMD	0	0	0%	
HIGH DENSITY	RHD	0	0	0%	0.00%
TOTALS		122.85*	769**	100.0%	100.0%

* - Acreage excludes public facilities, park, school(s), and neighborhood commercial.

** - Total units with residential option for CN would be 782.

3.5.1 Site Uses

Brighton Landing is a residential development and as such, the Permitted Uses, Conditional Uses and Prohibited Uses will be identical to those listed in the City of Vacaville Land Use and Development code (latest edition) for the various residential zone districts.

3.5.2 Density Standards

Density ranges for the various residential zone districts shall comply with the City of Vacaville Land Use and Development Code with the exception of the land use Residential Low Density, which may have a minimum density of 2.5 units per acre.

Floor area ratios shall comply with the City of Vacaville Land Use and Development Code except as noted in Table 3.3.

TABLE 3.3 – EXCEPTIONS TO MAXIMUM COVERAGE

Zoning District	Density	Maximum Coverage
RLD	3.1-5.0	0.45
RLMD	5.1-8.0	0.50
RMD	8.1-14.0	0.50

3.5.3 Building Standards

The standards for building height and front, rear and side yard setbacks shall comply with the City of Vacaville Land Use and Development Code except as noted in Table 3.4. (These exceptions are listed because nearly every project processed within the City of Vacaville has had these exceptions. The City should consider revising the Land Use and Development Code to reflect this.)

TABLE 3.4 –EXCEPTIONS TO BUILDING STANDARDS

Zoning District	Density	Minimum Front Yard to Habitable Space	Minimum Side Yard	Minimum Rear Yard	Minimum Distance Between Structures
RLD	3.1-5.0	15'***	5' & 10'*	15'	15'
RLMD	5.1-8.0	10'***	5' & 5'	15'	10'
RMD	8.1-14.0	10'***	5' & 5'	10'	10'

*- Footnote 5 of the Land Use and Development Code does not apply.

** - On corner lots the minimum setback from the angled corner shall be 8'.

3.5.4 Parking Standards

Parking shall be provided in accordance with City of Vacaville Land Use and Development Code, Chapter 14.09.128.

3.5.5 Other Development Standards

For Development Standards not specifically mentioned in this Specific Plan, refer to Chapter 14.09 Zoning in the City of Vacaville Land Use and Development Code.

3.5.6 Design Criteria

The developer of each phase (village) shall establish the architectural theme for their phase (village) and process this theme through the Planning Department using the Design Review process which will be subject to review and approval by the Planning Commission.

4.0 TRANSPORTATION AND CIRCULATION

4.1 INTRODUCTION

Goal: Provide a circulation system which maintains and improves access and connectivity in the area East of Leisure Town Road and within Brighton Landing.

Goal: Maintain the acceptable operation of Leisure Town Road (including plan to be segment of Jepson Parkway), and Elmira Road.

Goal: Highest level of capacity possible shall be maintained on Jepson Parkway and proposed "Major Collector", therefore no additional access to these roadways other than those accounted for in Specific Plan shall be considered.

Goal: Planning for school circulation and access, including but not limited to providing for anticipated arrival and dismissal traffic, shall be accomplished as part of facility designs so that impact to adjacent public streets is limited.

4.2 EXISTING SETTING

Brighton Landing is bounded by Leisure Town Road on the West, Elmira Road on the north, the PG & E power lines to the east and the Batch Property to the south.

The existing roadways and the proposed vehicular circulation system through the Specific Plan area are shown on Figures 4.1 and 4.2 and include the following:

4.2.1 Leisure Town Road (future Jepson Parkway)

Currently Leisure Town Road is a two lane rural road bordering the western portion of the area. Future plans call for this road to be redesignated as Jepson Parkway and will be a four lane arterial with large landscaped frontages. The developers of Brighton Landing will be responsible for dedicating the required right of way on the east side of the road and will install the sidewalk and landscaping on the east side of Jepson Parkway but will deposit funds with the City for the construction of curb, gutter, 20-foot

of pavement, street lighting, storm drainage, etc. The City will construct the street widening when the funds become available. See Figure 4.2 for the location and Figure 4.2.1 Leisure Town Road (Jepson Parkway) for the details of the proposed street section.

It should be noted that there continues to be concern that 4 lanes for Leisure Town Road (Jepson Parkway) will not be adequate in the future. To address that concern, the Brighton Landing Project has incorporated an additional 24 feet of landscaping along Leisure Town Road. So instead of the 35 feet of landscaping required by the Jepson Parkway Plan Line, the Brighton landing proposes a minimum landscape width of 59 feet. In addition, the final design of the trail/bike path will be located such that if the widening were needed in the future the path would not be affected.

4.2.2 Elmira Road

Elmira Road borders the Brighton Landing development on the north. Currently the road is two lanes but is designated to be a four lane arterial from Leisure Town Road to the Major Collector which bisects the Brighton Landing area. To the east of the Major Collector Elmira Road is designated as a 2-lane arterial. The southern portion of Elmira Road will be constructed by the respective adjacent development component of the Brighton Landing project to the interim configuration. See Figure 4.2 for the location and Figures 4.2.2 and 4.2.3 Elmira Road for the details of the proposed street section (both in the interim configuration and the ultimate alignment).

The transition from four lanes to 2 lanes will occur at the major collector through a lane drop in the east bound direction and through the addition of a lane west of the collector in the west bound direction.

4.2.3 Major and Minor Collector Roads with Trail

Within the Brighton Landing development there is one street section which is designated “Major Collector Road” and two street sections that are designated “Minor Collector Roads”.

The Major Collector Road is 56 feet wide which includes two 20 foot wide lanes and a 16 foot median. There are 10' wide concrete trails on both sides of the street and with no residential units fronting on the street. See Figure 4.2 for the location and Figure 4.2.4 Major Collector Road for the details of the proposed street section. Where the major collector intersects Elmira Road, a traffic signal may be warranted. If so, as a Condition of Approval for the Tentative Map, the developer will be required to fund 50% of the signal.

Minor Collector Roads are collectors (40 feet wide) with trails on one side of the street and some residential units fronting on the street. See Figure 4.2 for the location and Figure 4.2.5 Minor Collector Road for details of the proposed street section. One Minor Collector Road fronts the Linear Park on the east side of the Brighton Landing development area. Figure 4.2.6 shows the configuration of the Minor Collector at the Linear Park.

4.2.6 Residential Roads

All other streets not mentioned above will be designated as residential roads. Figure 4.2.7 illustrates the typical configuration of a residential street. This same figure also details the residential street along the south side of the private high school. Figure 4.2.8 illustrates the residential street adjacent to the trail which borders the southern boundary of Brighton Landing. One residential street within the Brighton Landing Project is slated to have separated sidewalks. This is illustrated in Figure 4.2.9. These streets will comply with the City of Vacaville standards for residential streets design.

4.3 CONNECTIONS

Completion of the street network within the Brighton Landing area will be the impetus for significant improvement in vehicular north south circulation for the eastern portion of Vacaville. Eventually Leisure Town Road will be improved and renamed Jepson Parkway. The facility along with the Major Collector paralleling Jepson Parkway will greatly improve traffic flow to and from Interstate 80. In addition, the completion of Jepson Parkway as envisioned by the STA will provide enhanced connections between Suisun City, Fairfield, Vacaville and Dixon.

4.4 VEHICULAR STREET NETWORK

The following are implementing policies to ensure completion of the Vehicular Street Network as envisioned by this Specific Plan.

4.4-P-1 The density of the residential development shall not generate traffic volumes that exceed the capacity (anticipated to be LOS D in the Updated General Plan) of the Leisure Town Road, Elmira Road, and the Major Collector Intersections.

4.4-P-2 The LOS throughout the remainder of the Brighton Landing area shall comply with the City's Traffic Mitigation Policy of not exceeding LOS C.

4.4-P-3 The City will require the Master Developer to enter into a Development Agreement to provide the necessary roadway improvements to serve the development and mitigate related traffic impacts.

4.4-P-4 Specific Plan developers shall fund circulation improvements necessitated by the Specific Plan development through various funding mechanisms (i.e. assessment districts, traffic impact fees, private funding, etc.).

4.4-P-5 The public road rights-of-way and street improvements shall comply with City of Vacaville standards and said rights-of-way will be dedicated to the City of Vacaville as public streets as shown in the following Figures:

4.2.1 Section A - Leisure Town Road (Jepson Parkway)

4.2.2 Section B – Elmira Road

4.2.3 Section B1 – Elmira Road

4.2.4 Section C – Major Collector Road

4.2.5 Section D & D1 - Minor Collector Roads

4.2.6 Section E - Minor Collector @ Linear Park

4.2.7 Section F & G - Residential Road

4.2.8 Section H – Road @ Southern Trail

4.2.9 Section I – Residential Street with Detached Sidewalk

4.4-P-6 *The traffic roundabouts shall comply with the FHWA latest design guidelines.*

4.5 NON-VEHICULAR CIRCULATION NETWORK

Goal: Develop a comprehensive bikeway/ pedestrian system that connect the park, school(s), and major arterials and promote the use of alternatives to driving.

4.5-P-1 *Construct the trail/bike system as shown in Figure 4.3 Non-Vehicular Circulation Diagram.*

4.6 TRANSIT

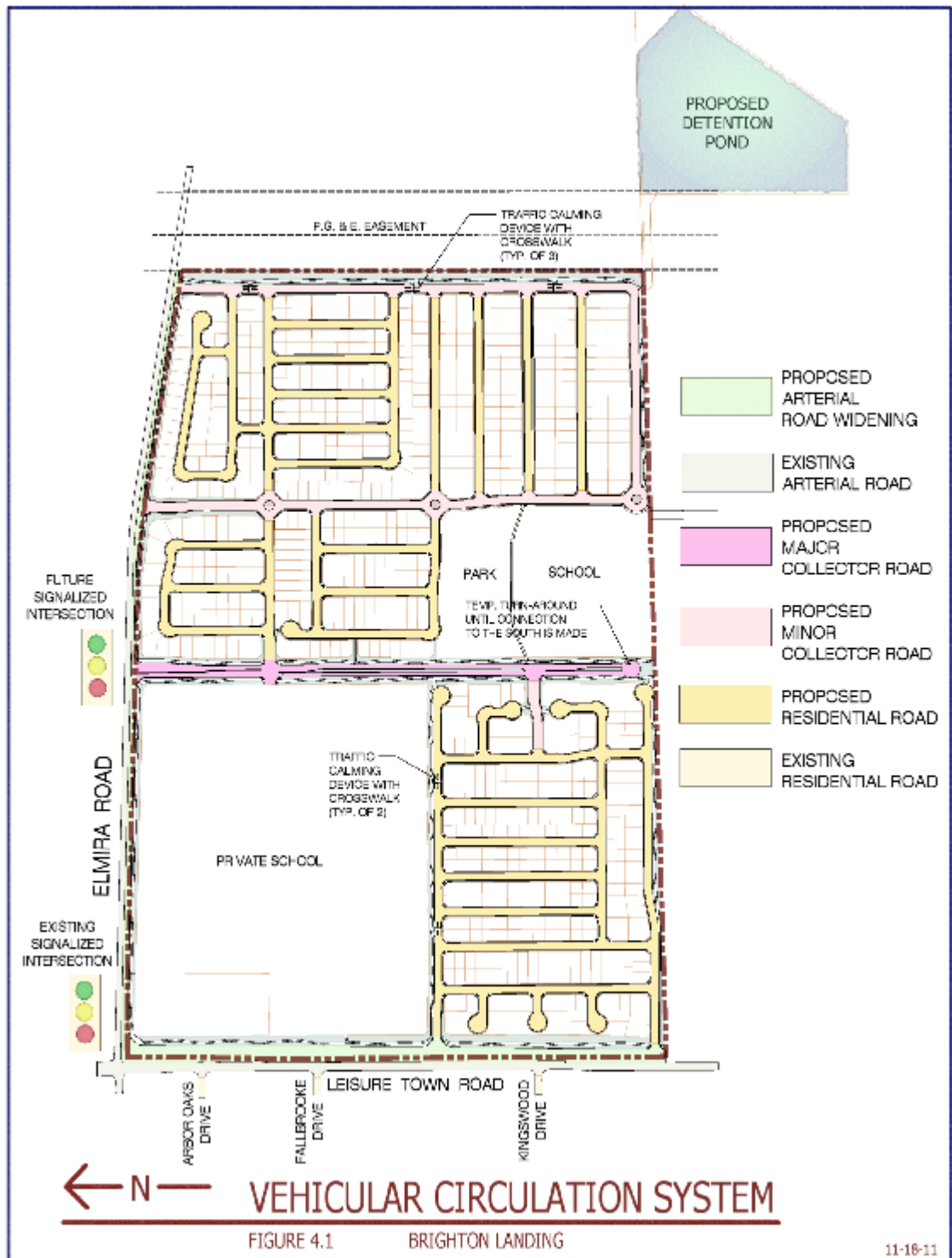
Goal: Promote increased local transit ridership and provide Specific Plan area residents with the opportunity to use public transportation as an alternative to driving.

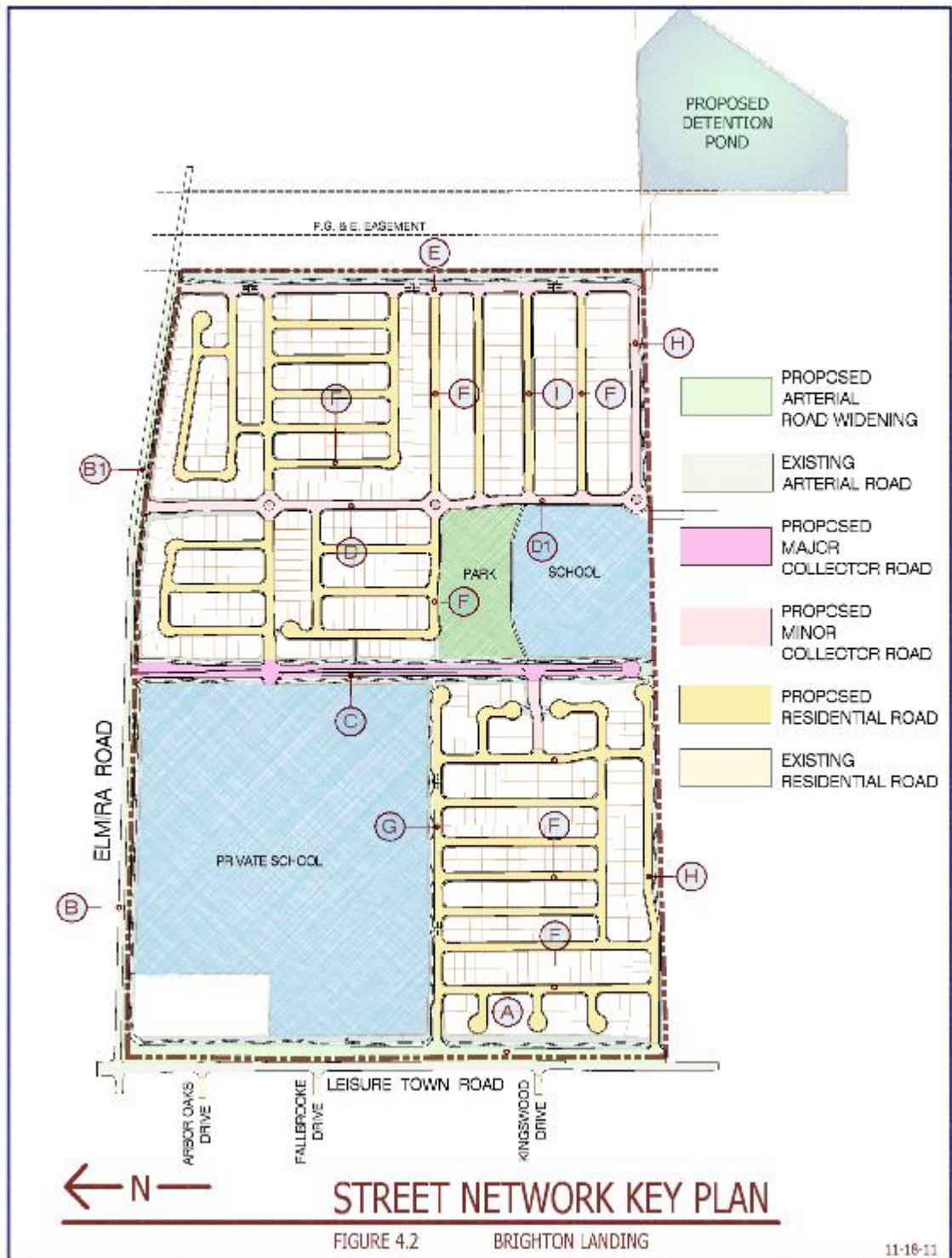
Transit service in Vacaville is provided by Vacaville City Coach through a fixed route and dial-a-ride services. City Coach services are coordinated with other area transit services for weekday peak hour commute service along Interstates 80 and 680 corridors. This includes services from the Fairfield/Suisun Transit and Vallejo Transit systems with routes that connect to BART stations in Pleasant Hill and El Cerrito.

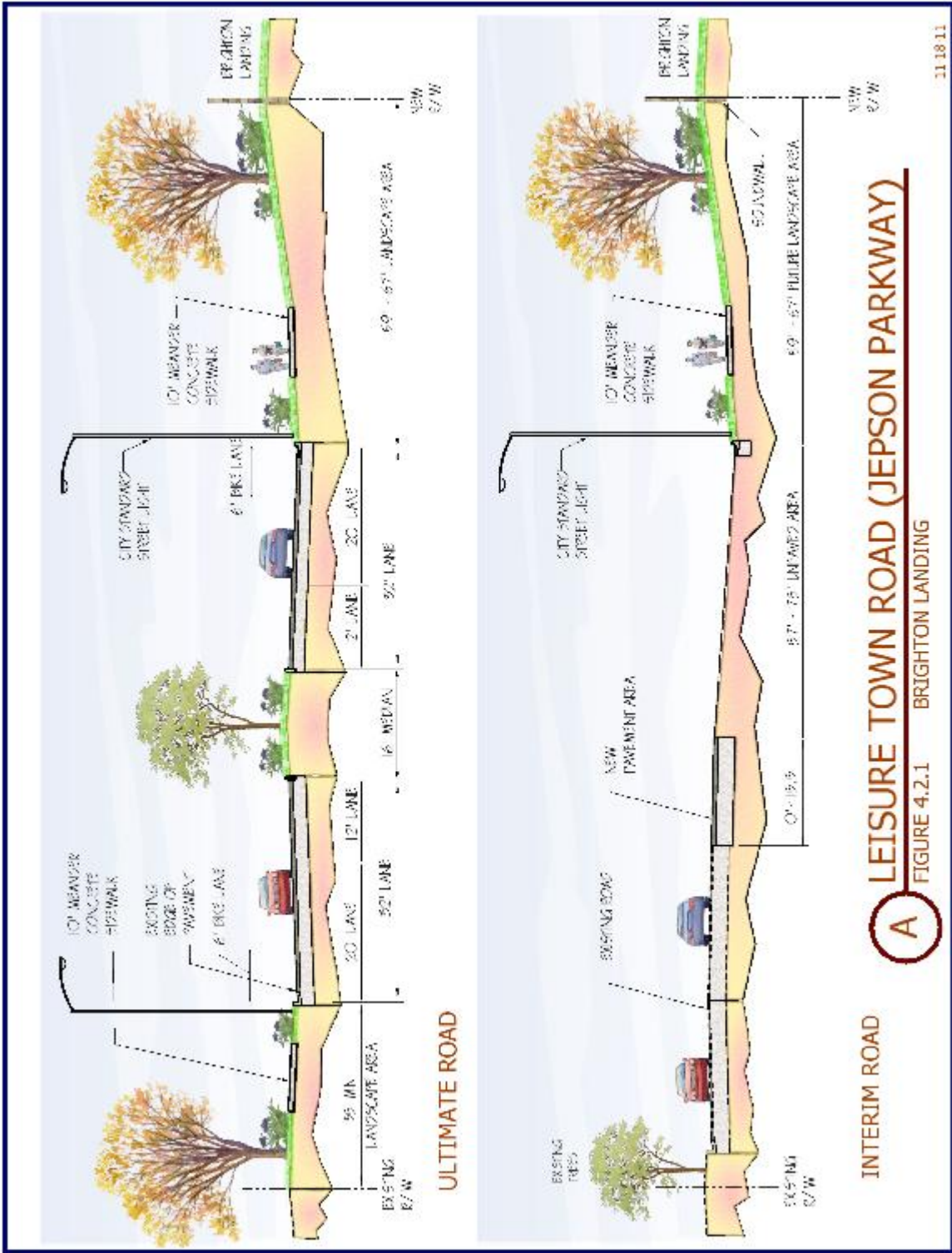
The following are implementing policies to meet the stated goal:

4.6-P-1 *Currently, no transit routes traverse Leisure Town Road in the vicinity of Brighton Landing. The City may wish to consider extending Route 4, 5, or 8 to Leisure Town to serve the Brighton Landing area.*

4.6-P-2 *If the City desires to expand Route 4, 5, or 8 (or any other route for that matter) into the Brighton Landing area the developers of Brighton Landing shall install bus turnouts and transit stops in location(s) designated by the City.*



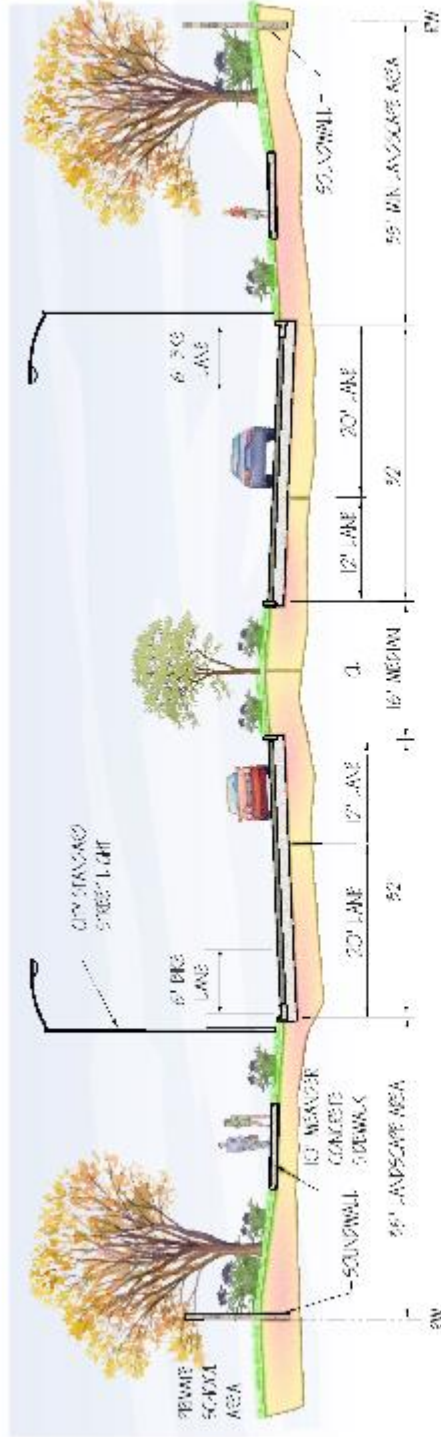




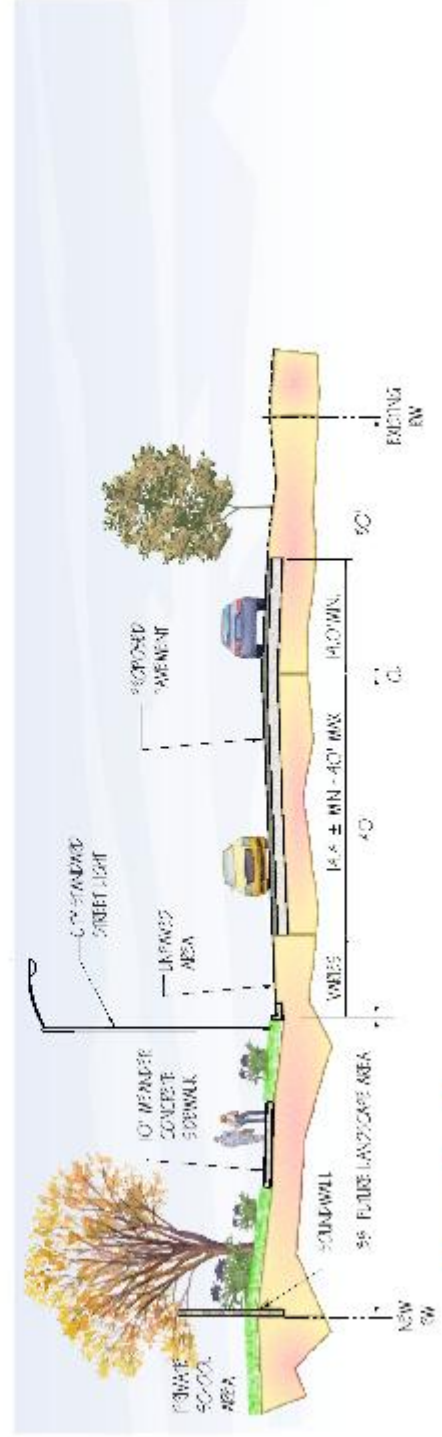
A LEISURE TOWN ROAD (JEPSON PARKWAY)

FIGURE 4.2.1 BRIGHTON LANDING

11.18.11

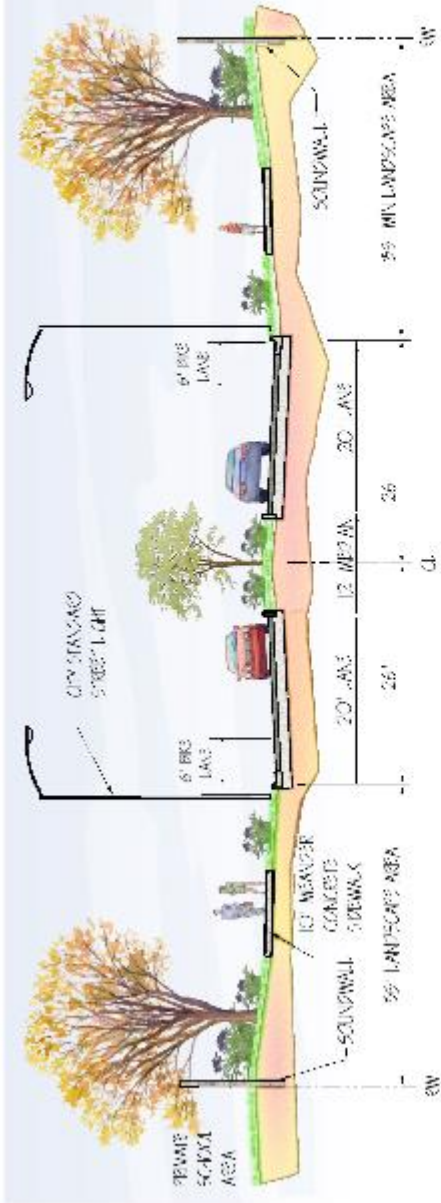


ULTIMATE ROAD

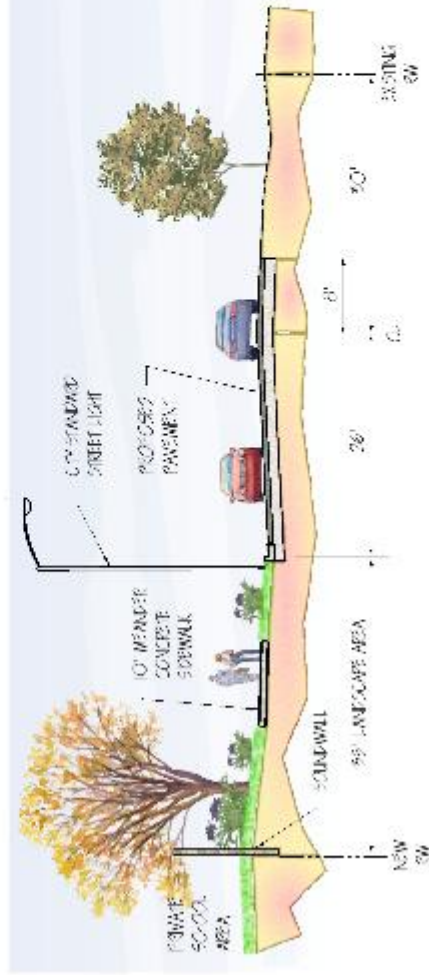


INTERIM ROAD

B **ELMIRA ROAD**
 FIGURE 4.2.2 BRIGHTON LANDING

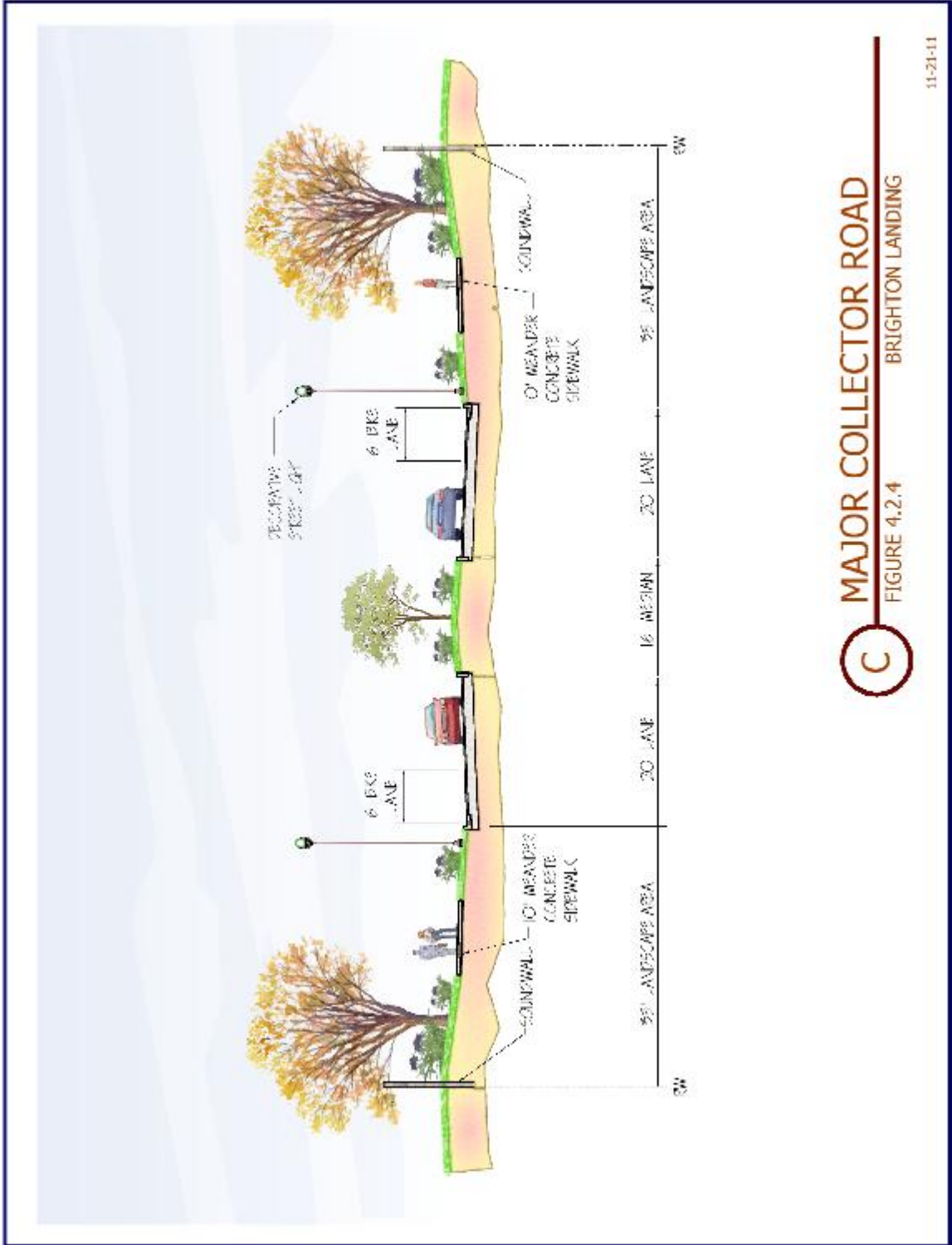


ULTIMATE ROAD



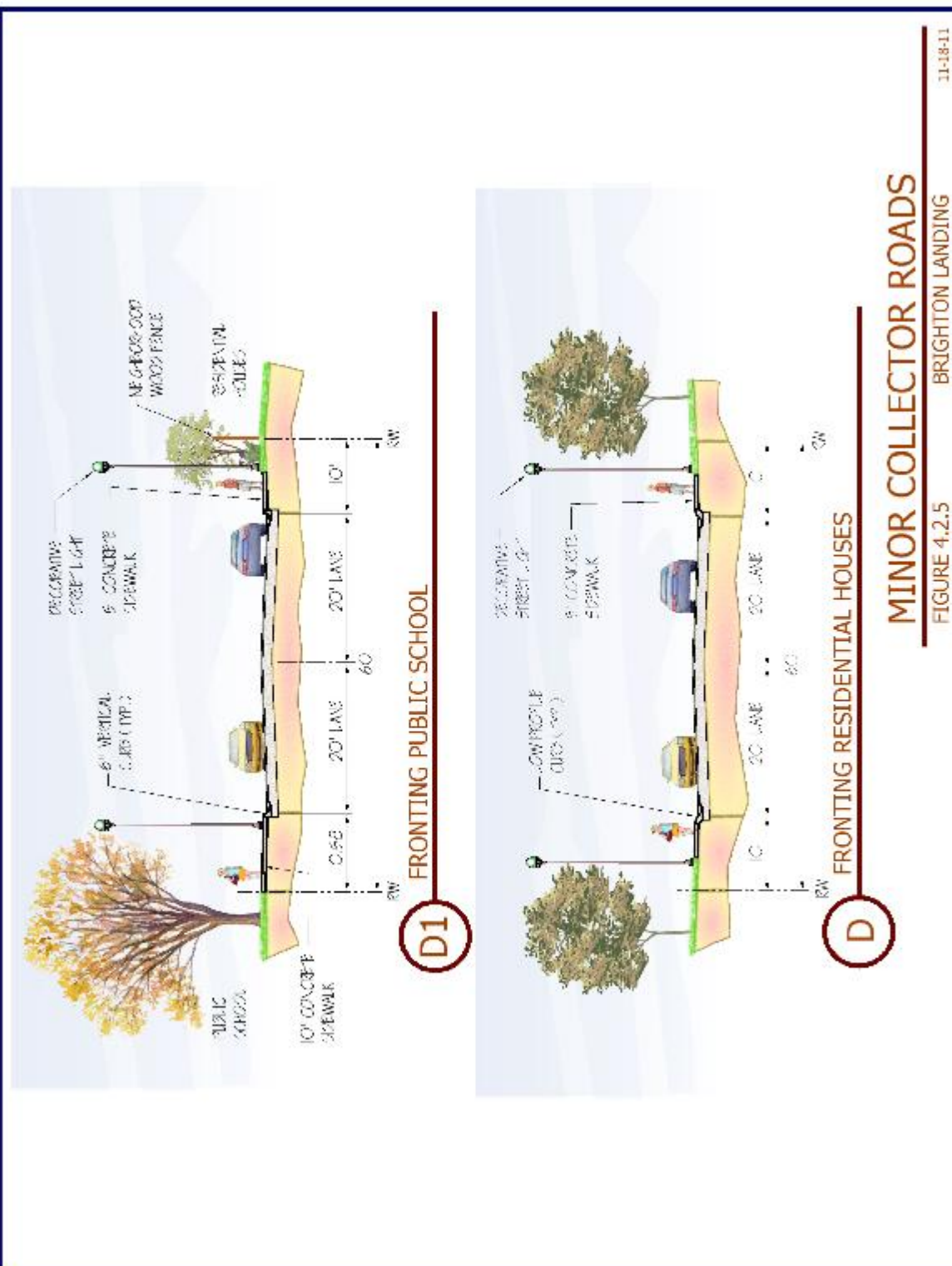
INTERIM ROAD

B1 **ELMIRA ROAD**
 FIGURE 4.2.3 BRIGHTON LANDING

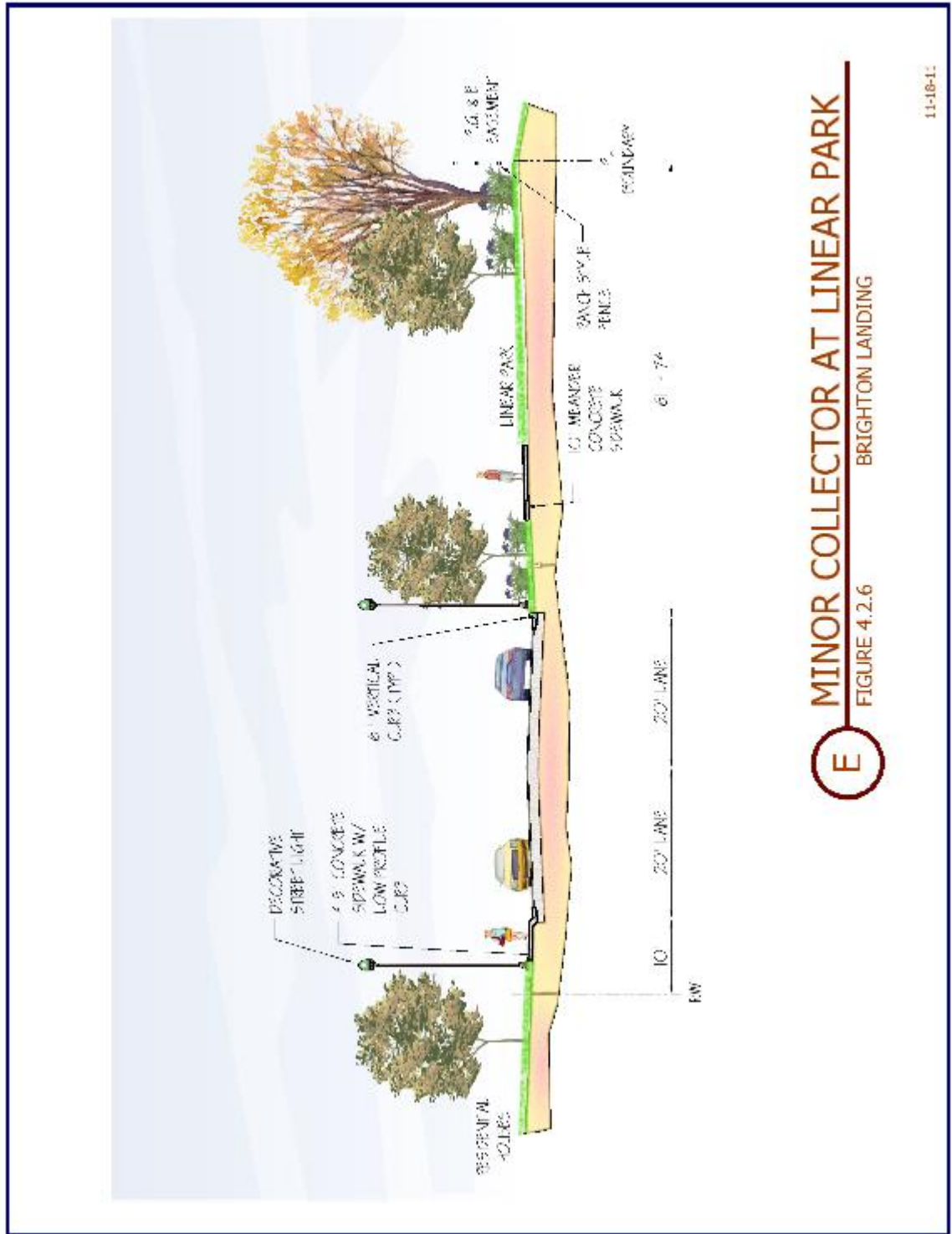


C MAJOR COLLECTOR ROAD
BRIGHTON LANDING
FIGURE 4.2.4

11-21-11



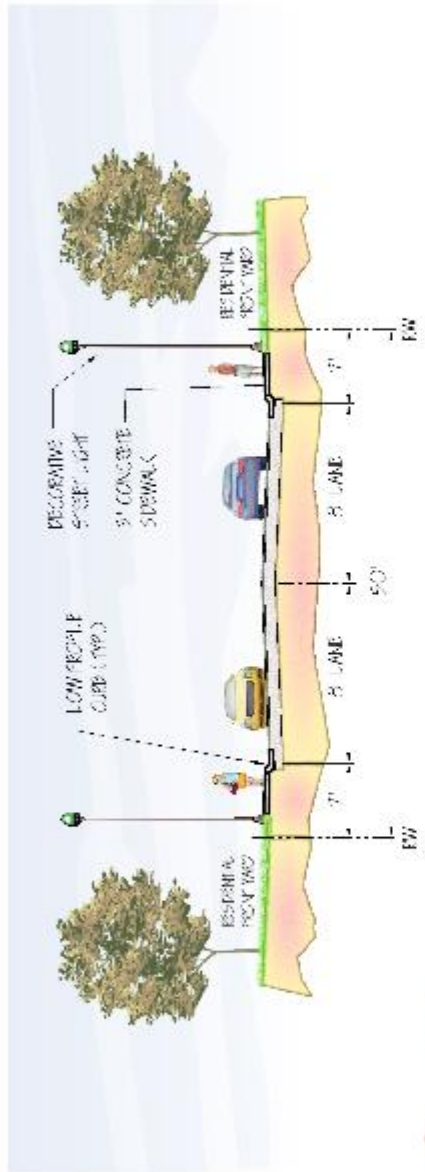
MINOR COLLECTOR ROADS
 BRIGHTON LANDING
 FIGURE 4.2.5



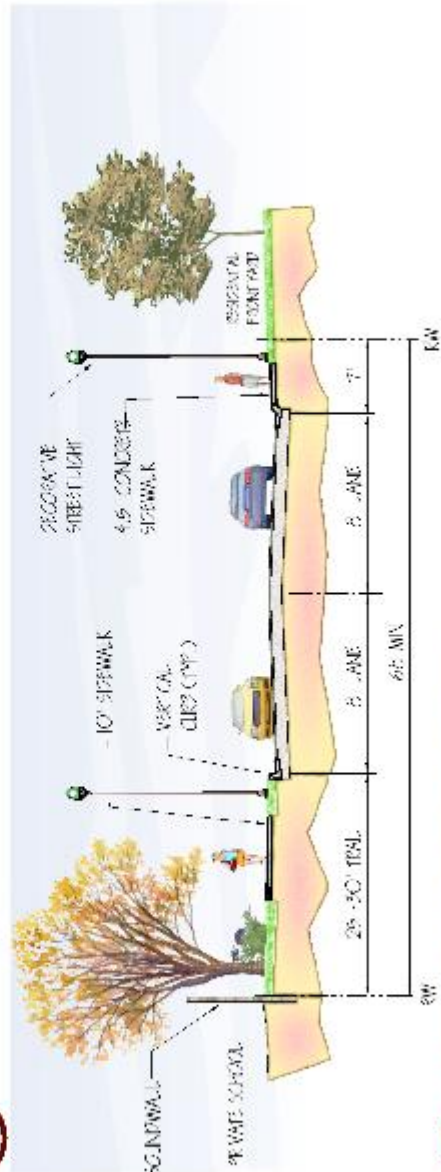
E MINOR COLLECTOR AT LINEAR PARK

FIGURE 4.2.6 BRIGHTON LANDING

11-18-11

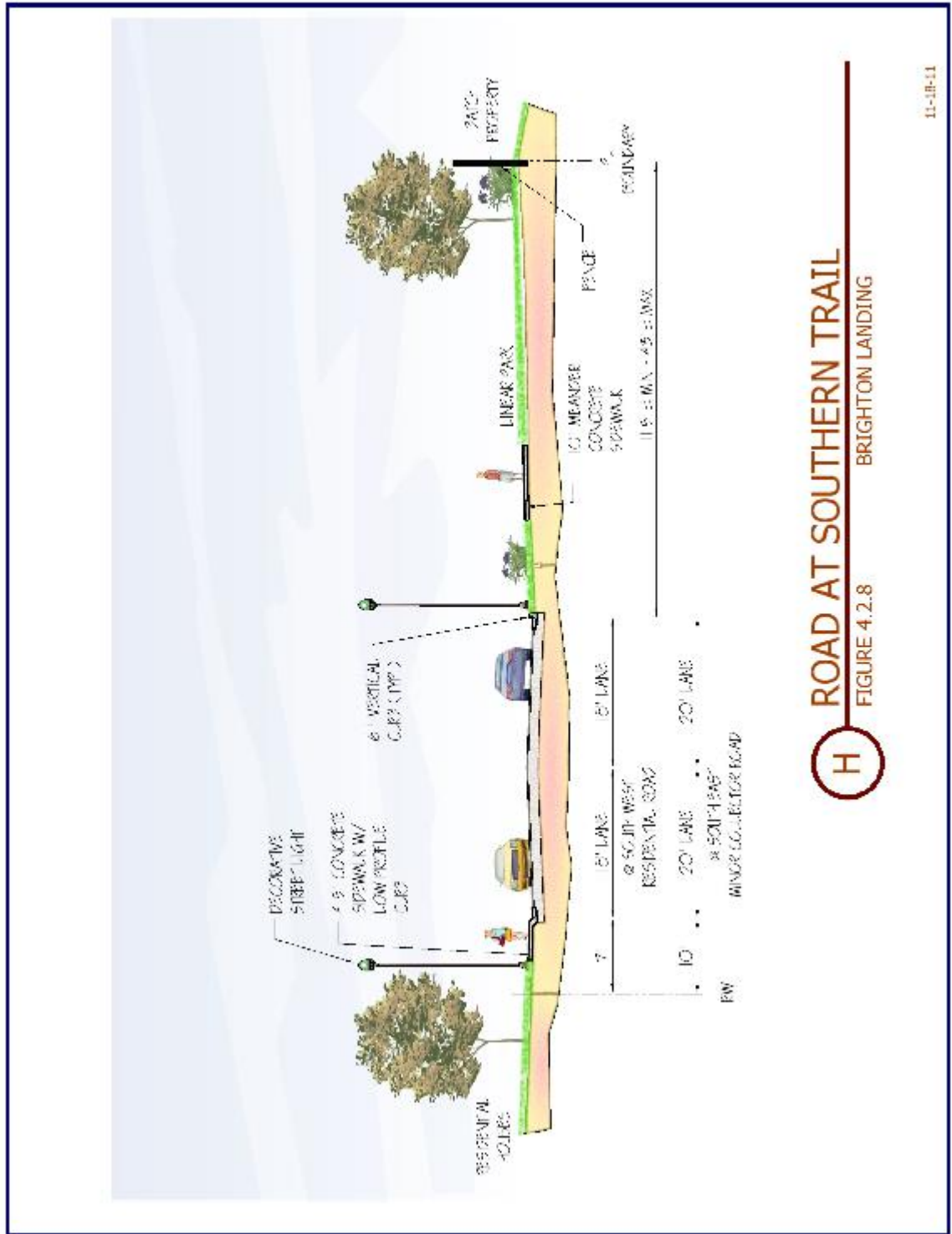


F TYPICAL

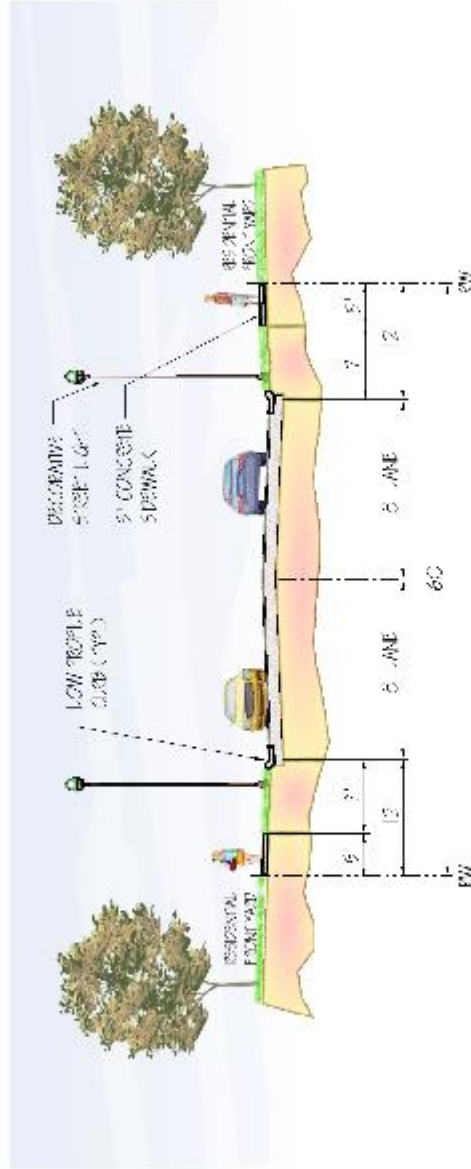


G ADJACENT TO PRIVATE SCHOOL

RESIDENTIAL ROAD
FIGURE 4.2.7 BRIGHTON LANDING

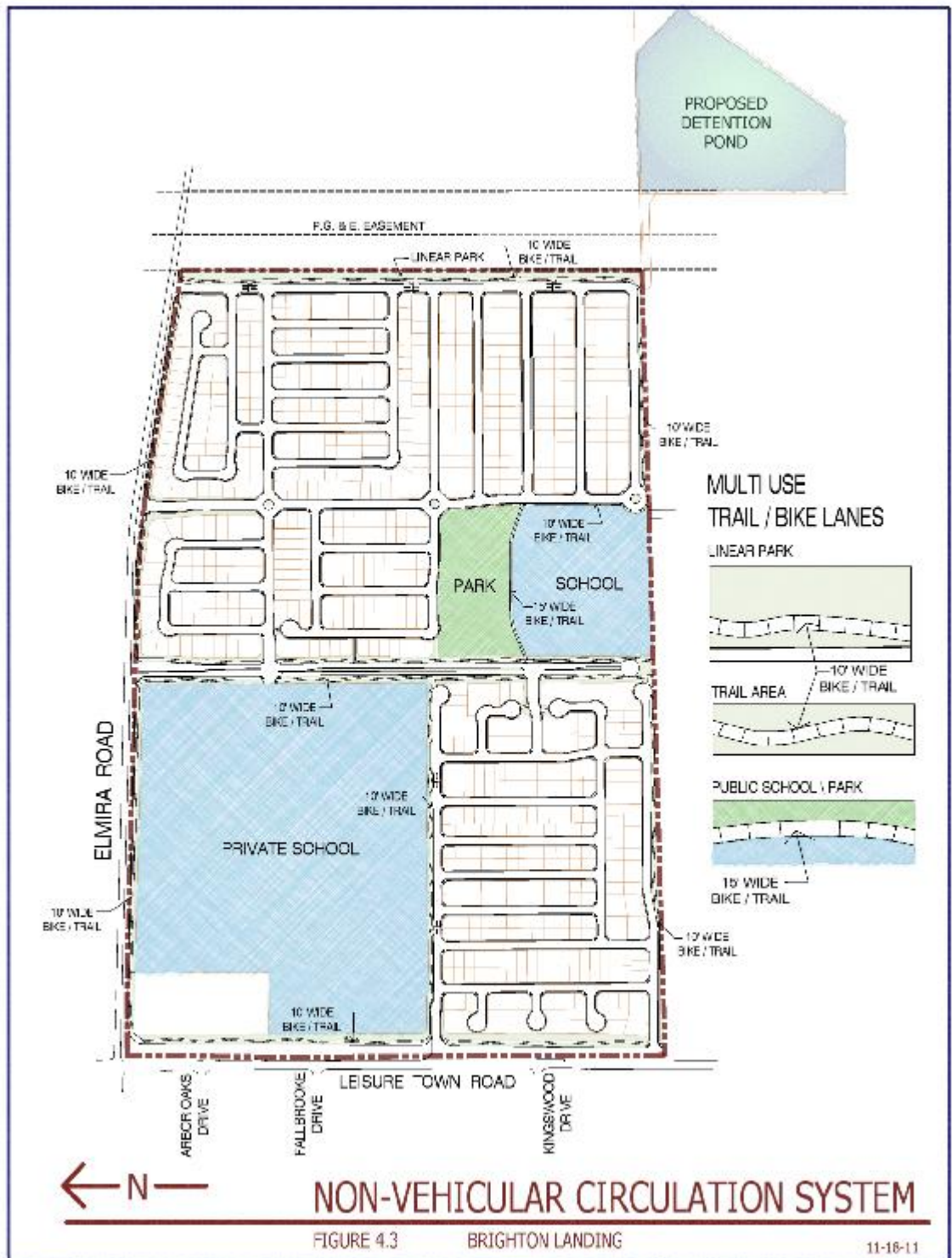


H ROAD AT SOUTHERN TRAIL
 BRIGHTON LANDING
 FIGURE 4.2.8



I RESIDENTIAL ROAD

FIGURE 4.2.9 BRIGHTON LANDING



5.0 RECREATION, OPEN SPACE AND RESOURCE MANAGEMENT

5.1 INTRODUCTION

- 1. Goal: To protect and enhance existing biological resources.**
- 2. Goal: To provide a development that is compatible with the Agricultural uses to the east.**

Currently a biological assessment is underway which will ascertain the existing biological resources, if any. Specific mitigations will be proposed if any endangered species or wetland features are impacted by the Brighton Landing development.

To minimize the impact of the project from Green House Gases (GHG) the houses shall be built using the “Build it Green” concept. In addition, to encourage electrical vehicles (and further minimize the GHG impact), each garage shall install the special plugs used by said vehicles.

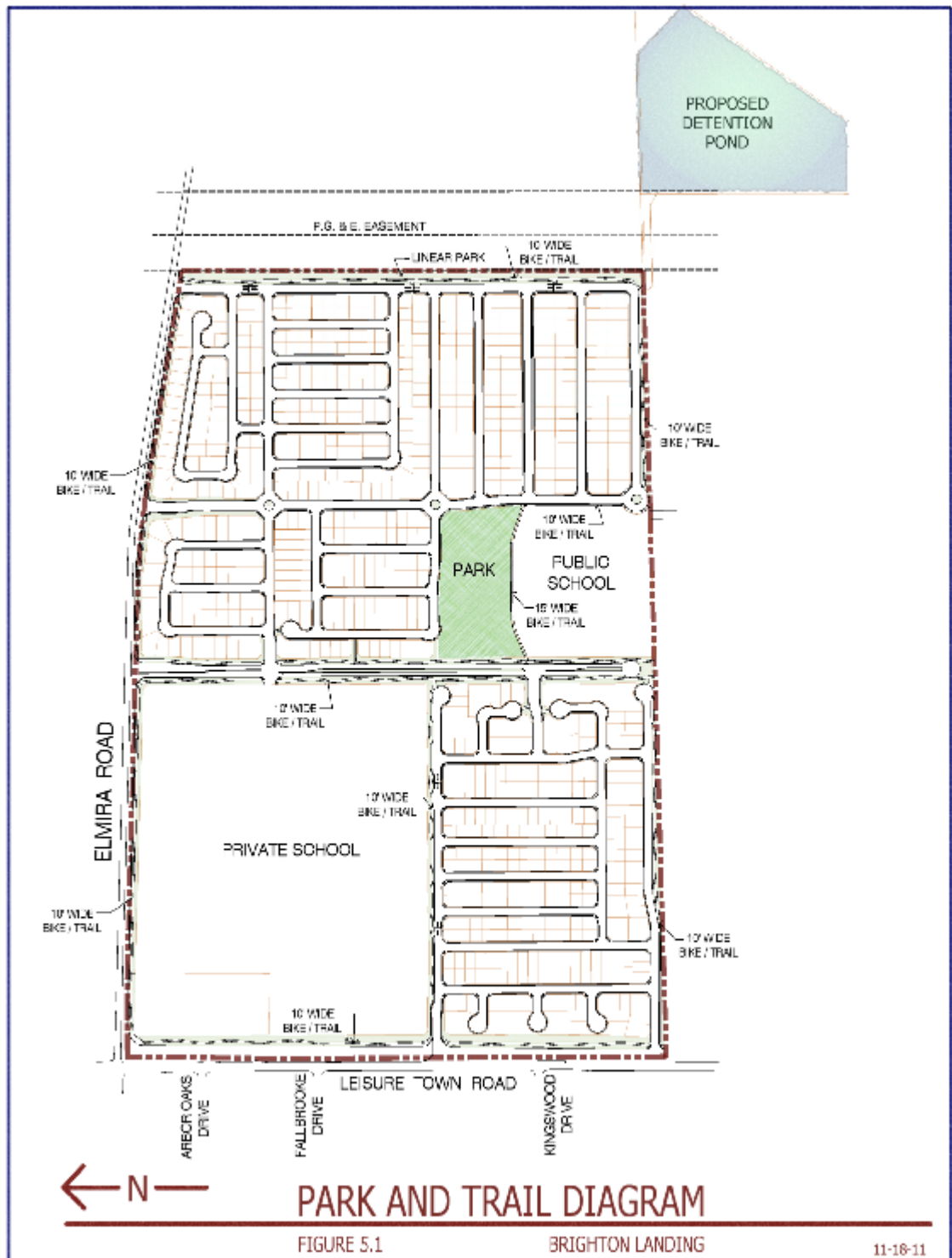
The Brighton Landing Specific Plan calls for a 500 foot buffer between the proposed residential and the existing agricultural uses east of the PG & E towers. This 500 foot buffer results in a large area under the power lines, a linear park/pathway along the eastern edge of the Brighton Landing area. With this buffer in place agricultural uses will not be impacted by the proposed project.

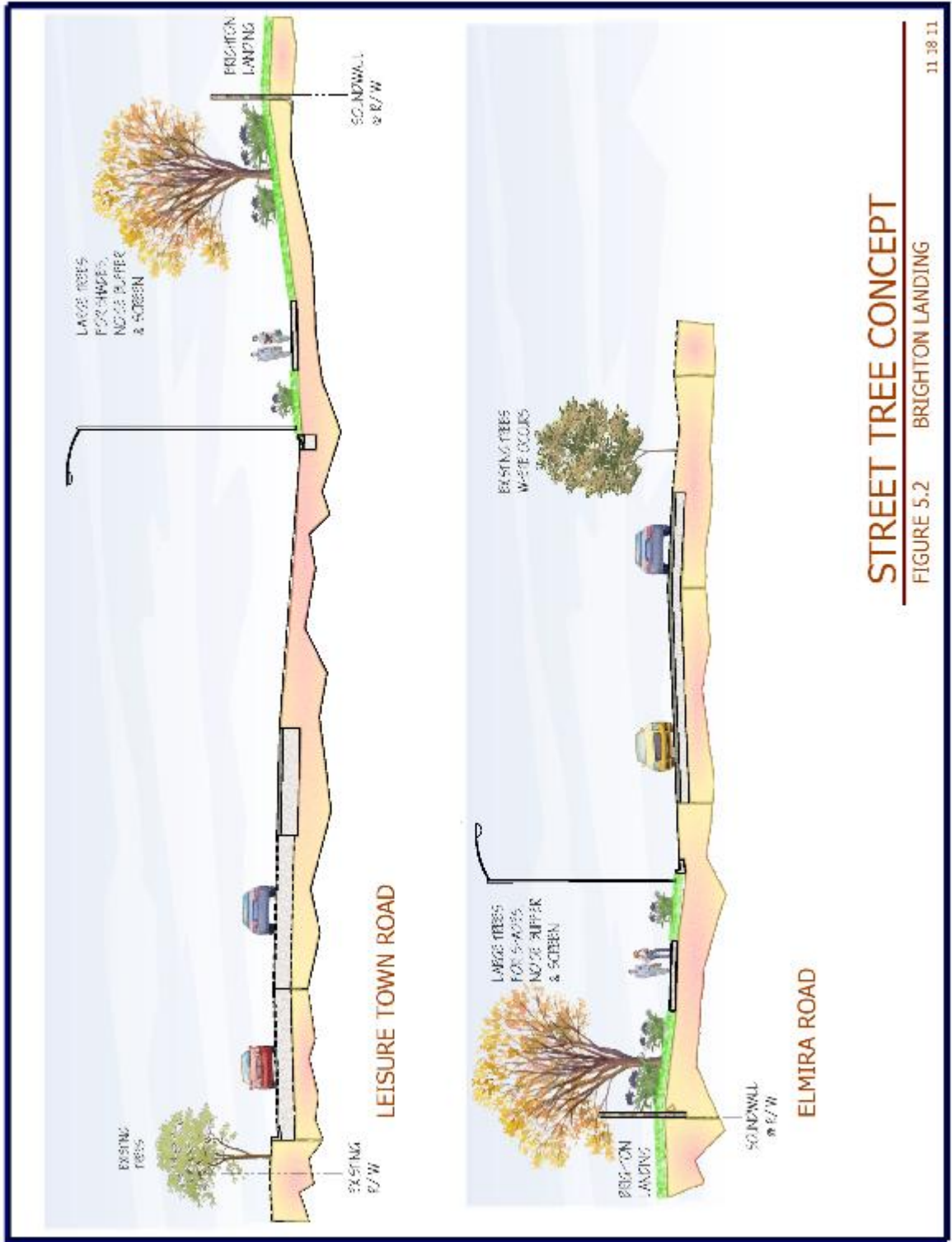
5.1-P-1 The area within the 500 foot buffer will be precluded from future development by virtue of the fact that the power line easement comprises a large portion of the area with the balance of the buffer being dedicated to the City as either a linear park/trail or street right of way which will ensure a permanent buffer between Brighton Landing and the agricultural uses to the east. The maintenance will be paid for by the residents of Brighton Landing through the use of a Lighting and Landscape District (LLD). The buffer will not be used for intense public uses. Appropriate uses within the buffer include detention basins and solar energy projects. Preservation of the buffer shall be through means of a dedicated easement or fee title.

5.2 PUBLIC OPEN SPACE AND RECREATION

As mentioned in Section 2; development of the Brighton Landing area will result in the construction of a new 6 acre park adjacent to the proposed school site. The City of Vacaville and the Vacaville Unified School District are encouraged to have a joint use agreement regarding this park to increase its usability and functionality. In addition to the new park, a series of trails will be constructed throughout the development which will connect with the park, and the schools. The Brighton Landing project will be constructing approximately 3.11 miles of trails and when combined with the park and landscaping, a total of 21.07 acres are dedicated to open space and recreational area. Street trees will also be planted throughout the project area. Figures 5.2 and 5.3 illustrate the street tree concept in various locations.

5.2.1-P-1 The park, trails, and backup landscaping will be designed to enhance the Brighton Landing development area. High quality material will be used along with drought tolerant plants in conformance with the City of Vacaville landscape ordinance. The park, trails and back-up landscaping will be dedicated to the City of Vacaville as public amenities with maintenance paid for by the residents of Brighton Landing through the use of a LLD.

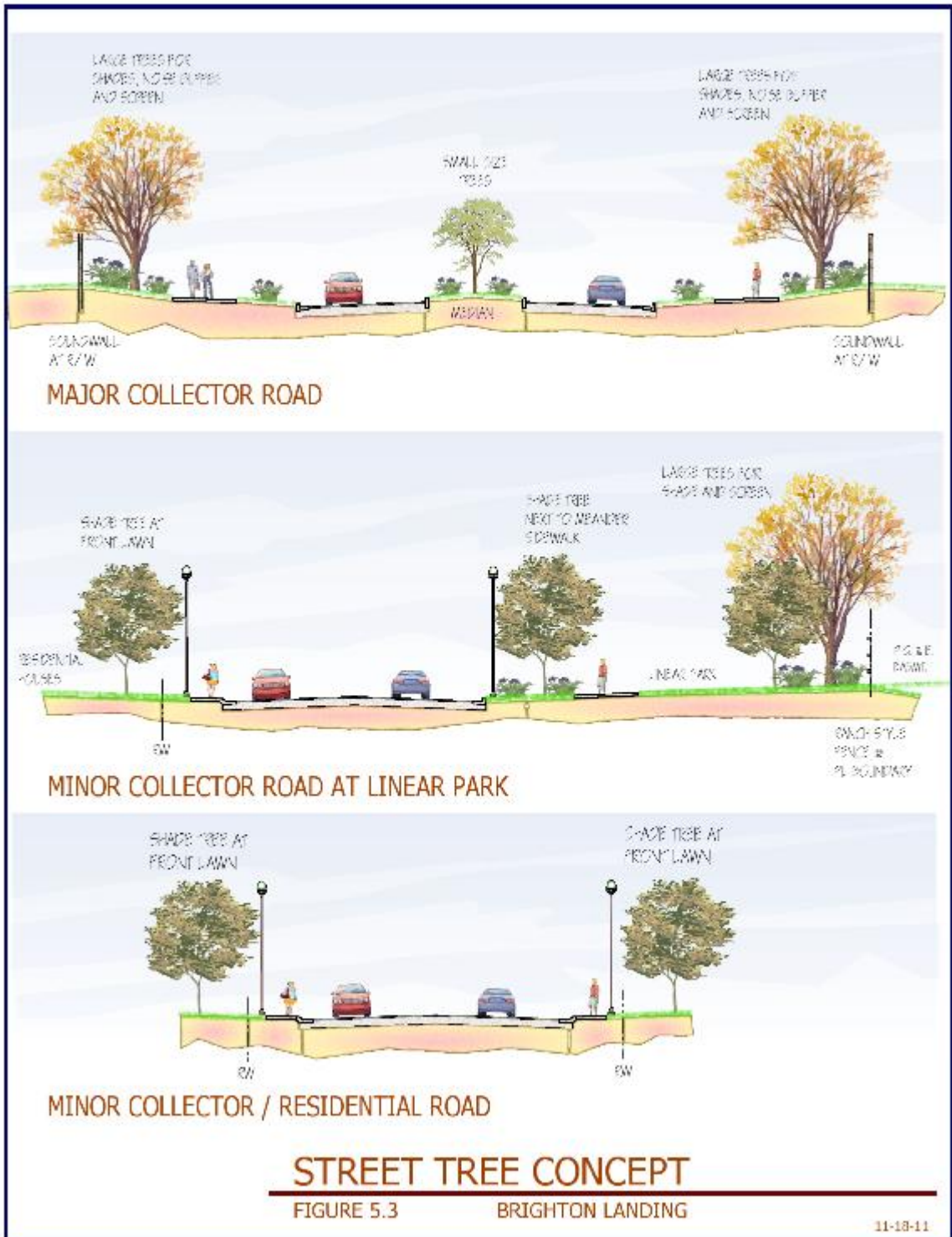




STREET TREE CONCEPT

FIGURE 5.2 BRIGHTON LANDING

11.18.11



6.0 COMMUNITY DESIGN

6.1 INTRODUCTION

- 1. Goal: Create walkable neighborhoods with interconnected streets and trails that encourage pedestrian activity.**
- 2. Goal: Construct a neighborhood park which connects to the trails and is adjacent to the schools.**
- 3. Goal: Construct a regional high school serving 1,200 students.**
- 4. Goal: Construct a public school (anticipated capacity is 650 for an elementary school and 1,100 if expanded in the future to serve as a K-8 school) that serves the needs of the local community.**

6.2 PURPOSE AND INTENT

6.2-P-1 Create an attractive residential setting that compliments surrounding residential development to the west and agricultural uses to the east while setting the stage for future development east of Leisure Road both north and south of Brighton Landing.

6.2-P-2 Create a suitable buffer between the Brighton Landing development and the agricultural uses to the east.

6.2-P-3 Provide design guidelines to ensure that Brighton Landing is an attractive setting for Vacaville residents.

6.3 COMMUNITY DESIGN THEME:

“LIVABLE NEIGHBORHOODS”

The intent of the Brighton Landing development is to create neighborhoods that families can afford. To that end, the development encourages a variety of housing types including a significant number of small single family homes on small lots.

6.3-P-1 House design and materials are intended to be of high quality while recognizing the need to keep homes affordable. The

choice of materials, colors and detailing shall be thoughtfully integrated into the design.

6.3-P-2 Neighborhoods should be created with similar design themes and elements to create strong neighborhood identities.



6.4 BUILDING FORM

The houses should be designed creating a sense of variety. This can be accomplished by utilizing both one and two story elements on the same building and by varying roof plans. In general the community's architecture should create a pedestrian friendly street experience, create neighborhood identity and recognize the need for more affordable living.

6.4-P-1 There should be a wide range of product types incorporated into the various neighborhoods.

6.4-P-2 Garage doors should not become the dominant element of the front elevation. Relief should be achieved by recessing doors and by varying the garage orientation (recessed, swing-in, side street access, etc.).

6.5 STREETScape CHARACTER

Streets will be planted with approved street trees and enhanced streetscape landscaping as defined in subsequent submittals. Streetscapes should reflect the identity of the roadway system. Taller more imposing trees and widened (20-35') landscape strips should define the arterials and major collectors. Medium-sized trees and smaller (5-20') landscape strips should articulate the minor collectors. In general street trees should be used consistently on residential streets to establish attractive neighborhoods.

6.6 ENTRY FEATURES

Entry features should be located at four key locations on the exterior of the Brighton Landing development area (in each case the developer closest to the designated feature will be responsible for its construction). Refer to Figure 6.1 Community Entry Features Locations:

1. On the north side of Leisure Town Road at the intersection of the road at the center of the Brighton Landing development area.
2. On the south side of Elmira Road at the intersection of the street closest to the PG & E towers near the eastern end of the development area.

3. On the east side of the Major Collector at Elmira Road.
4. On the west side of the Major Collector at the southern most street that intersects the Major Collector.

6.6-P-1 The developers of the Brighton Landing project will design and install entry features at the locations designated above. The design will include high quality elements that comply with the City of Vacaville Gateway ordinance. The design for the first entry feature will be used for subsequent features.

6.7 SCHOOLS

An 11.0 acre site has been set aside for the construction of an elementary school serving the Vacaville Unified School District. At the time of the preparation of this Specific Plan no decision has been made by the School District regarding the required school configuration. The 11.0 acre site as configured with the Brighton Landing development can be developed as an elementary school. If the District chooses to construct a K-8 school or a middle school the Brighton Landing site will serve as the northern half of the ultimate school. The southern half of the school site would be on the Batch Property. The PLUA provides for this southern half of the school on the Batch property.

The Vacaville Unified School District, with input from the City of Vacaville and the developers of the Brighton Landing project, will design and construct a school that is in keeping with the character of the Brighton Landing development area to serve the needs of the residents of east Vacaville. Developer impact fees generated by development east of Leisure Town Road will fund construction of the school.

In addition to the public school site a 50 acre site is set aside for the construction of a private regional Catholic High School. The school would be sized to serve approximately 1,200 students and faculty. The school design will require City of Vacaville approval and the design and character of the school facility should be in keeping with the overall design of the Brighton Landing development.

Both schools will require City of Vacaville water and sewer services to develop. Road improvements should be designed to facility to heavy traffic flow expected during morning and afternoon peak hours when students are being arriving or leaving school. The Vacaville Unified School District and the Catholic High School operators are encouraged to coordinate schedules to minimize the impacts the schools may create for each other (i.e. stagger starting and dismissal times).

7.0 PUBLIC FACILITIES

7.1 INTRODUCTION

This section of the Specific Plan provides both general and specific guidelines for the development of future infrastructure facilities necessary to serve the Brighton Landing development area. The infrastructure facilities covered in this section include water distribution (potable and non-potable), sanitary sewer collection, storm drainage and detention, gas, electric, telephone, and cable TV.

The time frames for installation of various public facilities will be established through the adoption of a Brighton Landing Benefit District. This District will spell out which improvements will be required with each phase of construction. This is discussed in greater detail in Section 9.3.

This section's specific purpose is to:

1. Ensure that the project incorporates adequate water and sewer facilities to meet the needs of the Brighton Landing area.
2. Use non-potable water in Brighton Landing to reduce demand for potable water.
3. Size a detention basin to mitigate for increased run-off due to development.
4. Present preliminary sizes of utility components. Final pipe sizes will be based on computer modeling using the City wide models maintained by the Utilities Department and funded by the developers and will be subject to the approval by the Director of Utilities prior to design approval. On-site piping and utility facilities shall be sized to accommodate only the development in the Specific Plan area.
5. Identify off-site improvements which are necessary to serve the project.

Goal: Ensure that project design incorporates adequate infrastructure facilities to meet City design standards and the needs of the Brighton Landing residents.

Goal: To the extent required by law, use non-potable water and water conservation practices in the Brighton Landing area to reduce demand for potable water.

7.2 WATER SUPPLY

Goal: Provide an adequate and reliable source of potable and non potable water for the Brighton Landing Specific Plan area.

7.2.1 Potable Water Supply

Figure 7.1 shows the Potable Water Specific Plan with existing and proposed water line sizes. In general, Leisure Town Road has an existing 12" water main and it is intended that there will be 12" water mains in all arterials and collectors. In addition, a 12" water main is envisioned for the Major collector to serve as a future water main for development east of Leisure Town Road. Streets surrounding the high density site will require 12" water mains. All other streets will contain an 8" water line with domestic services and fire hydrants as required to serve the development.

7.2-P-1 Each developer will be responsible for construction of their portion of the water lines shown on the Potable Water Specific Plan. Each developer will be required to demonstrate that the proposed phasing of water line installation will provide for adequate redundancy (looping) and flow to the satisfaction of the Director of Public Works.

7.2-P-2 Adequacy of the water distribution system needs to be confirmed in each phase up to and including build-out to the satisfaction of the Director of Utilities.

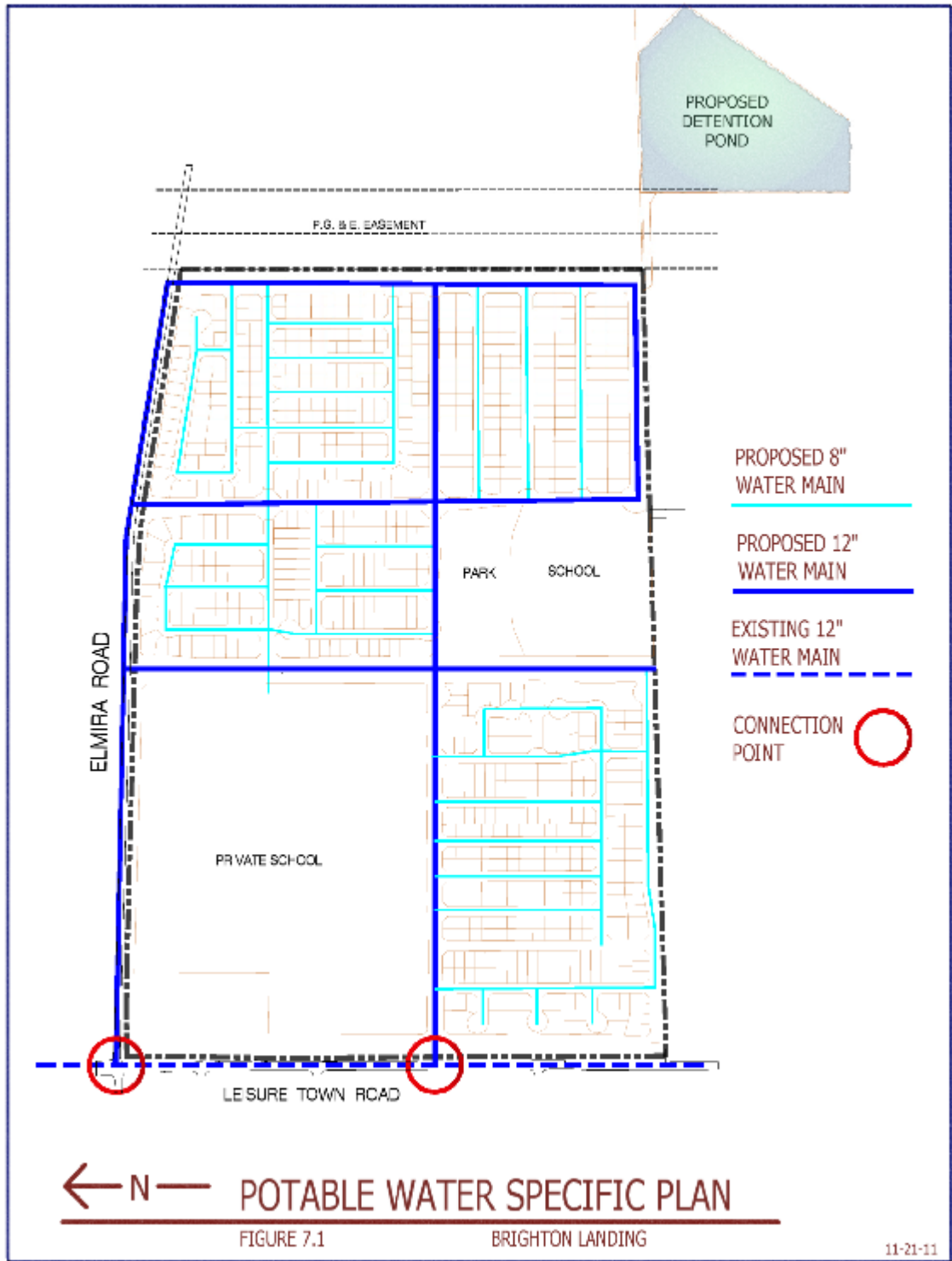
7.2-P-3 Developers will use low-flow irrigation systems and drought resistant plant materials to the extent that it is required by law.

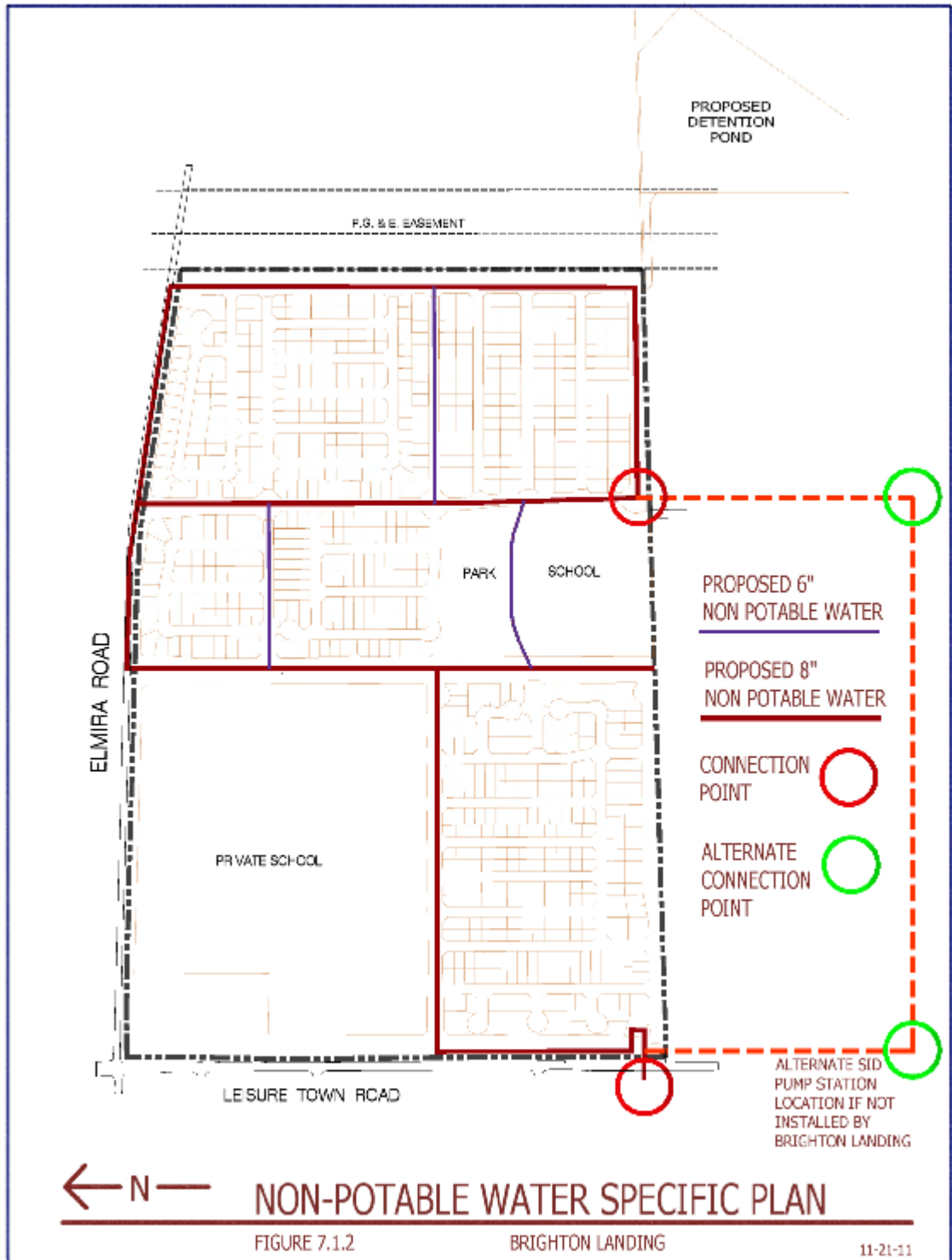
7.2.2 Non Potable Water Supply

Figure 7.1.2 shows the Non Potable Water Specific Plan with existing and proposed water line sizes. In general, 8" water mains are to be located in

all arterials and collectors. On Leisure Town Road it is envisioned that a Solano Irrigation District (SID) pump will be required. The 8-inch non potable water mains will be constructed and each phase proceeds.

The SID Pump Station will be installed no later than when the 400th building permit is issued. Prior to the installation of the pump station, landscaping will be irrigated with potable water through the use of temporary connections. The construction costs for the pump station will be a component of the Brighton Landing Benefit District. The pump station will be located at the southwest corner of the project adjacent to Leisure Town Road and the trail. Final detailed plans will be processed with the initial Large Lot Tentative Map.





7.3 SEWER

Goal: Provide an adequate and reliable wastewater collection system for the Brighton Landing Specific Plan area.

Figure 7.2 shows the Sewer Specific Plan with existing and proposed sewer line sizes. In general, all streets will have a minimum of an 8" line. It is envisioned that the Brighton Landing development will be key in developing a sewer system that will serve the entire area of future development east of Leisure Town Road. Preliminary analysis of the required sewer main line indicates that a new 36" trunk main from the Easterly Waste Water Treatment to and through Brighton Landing will be required. The depth of the sewer line is also critical to insuring that development can occur without significant importing of soil to raise the property to maintain adequate sewer cover. Beginning a new sewer system at the treatment plant allows for that design.

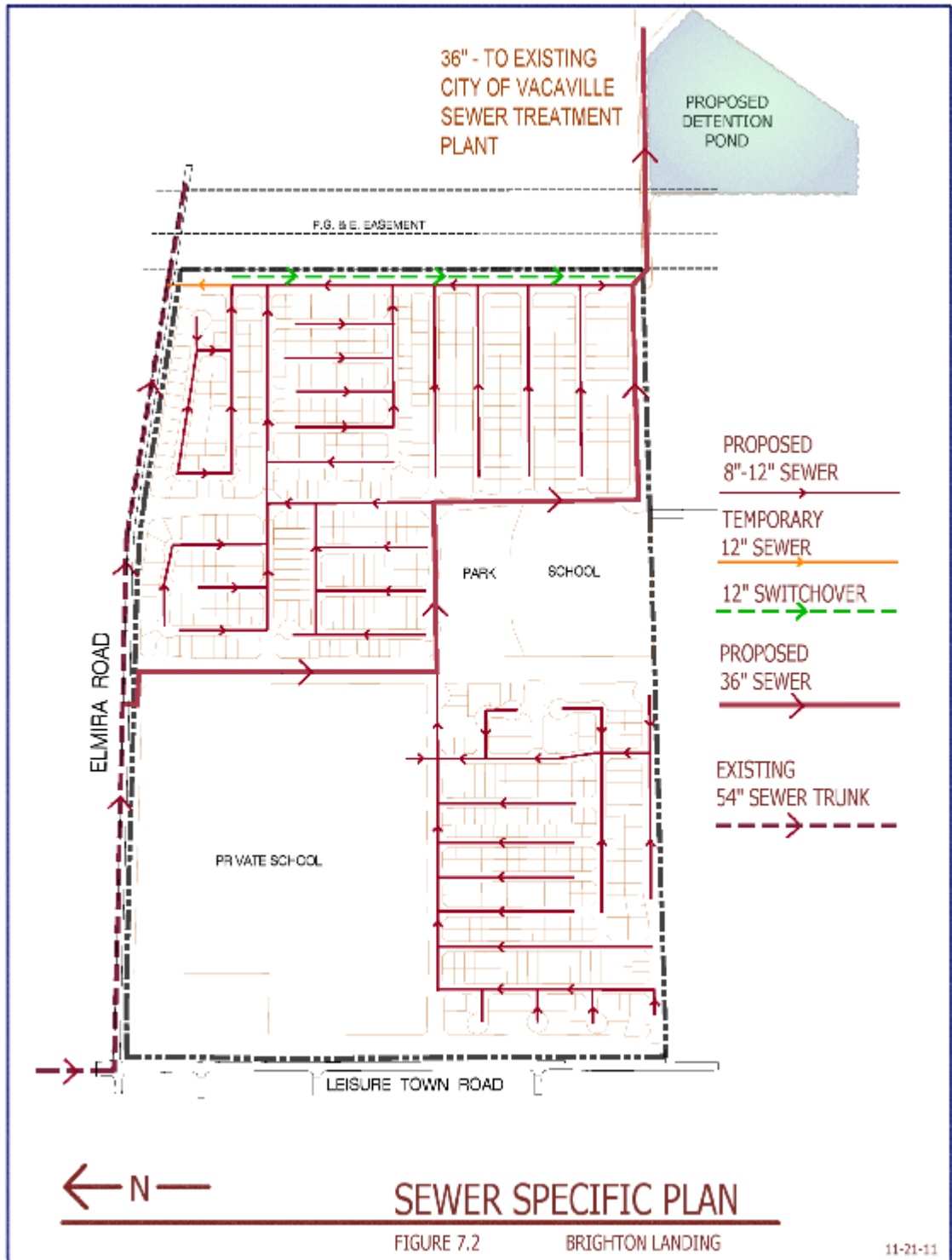
The 36" trunk sewer line is in excess of what the Brighton Landing project would be required to construct as it will potentially serve approximately 3,000 +/- acres of development. The size of the 36" sewer may change based on final analysis of the Preferred Land Use Alternative. Further, it is recognized that development of the sewer system beyond Brighton Landing will likely require the formation of a Sewer Assessment District or Benefit District. It is expected that the Brighton Landing Project would participate in the formation of the District which would result in the Brighton Landing developers being reimbursed for the portion of the sewer system installed that is in excess of their obligation. The exact dollar amounts would be worked out in the formation of the Assessment or Benefit District.

To reduce the upfront costs associated with development of Brighton Landing, discussions have occurred with the Public Works Department to construct a temporary connection to the 54" sewer main in Elmira Road. This temporary connection would be used for up to 385 housing units. The developers of the first 385 units would be required to contribute funds to be set aside for the construction of the ultimate sewer improvements. It is expected that after 385 housing units are constructed, a 12" sewer line will have been installed underneath the east boundary trail which will facilitate

disconnecting the sewer into the 54" and allow for the flow to be transmitted to the new 36" sewer main. The proposed "switchover" line is shown in green on Figure 7.2.

Further, to the extent that sewer improvements are installed that are part of the existing City sewer master plan (i.e. projects that been identified as components of the City's sewer impact fees); fee credits will be available.

7.3-P-1 Each developer will be responsible for construction of their portion of the sewer lines shown on the Sewer Specific Plan (Figure 7.2). The Brighton Landing Benefit District will be created which will identify timing of improvements and costs that must be set aside to demonstrate adequacy of each phase of the system up to and including full build-out to the satisfaction of the Director of Utilities.



7.4 STORM DRAINAGE

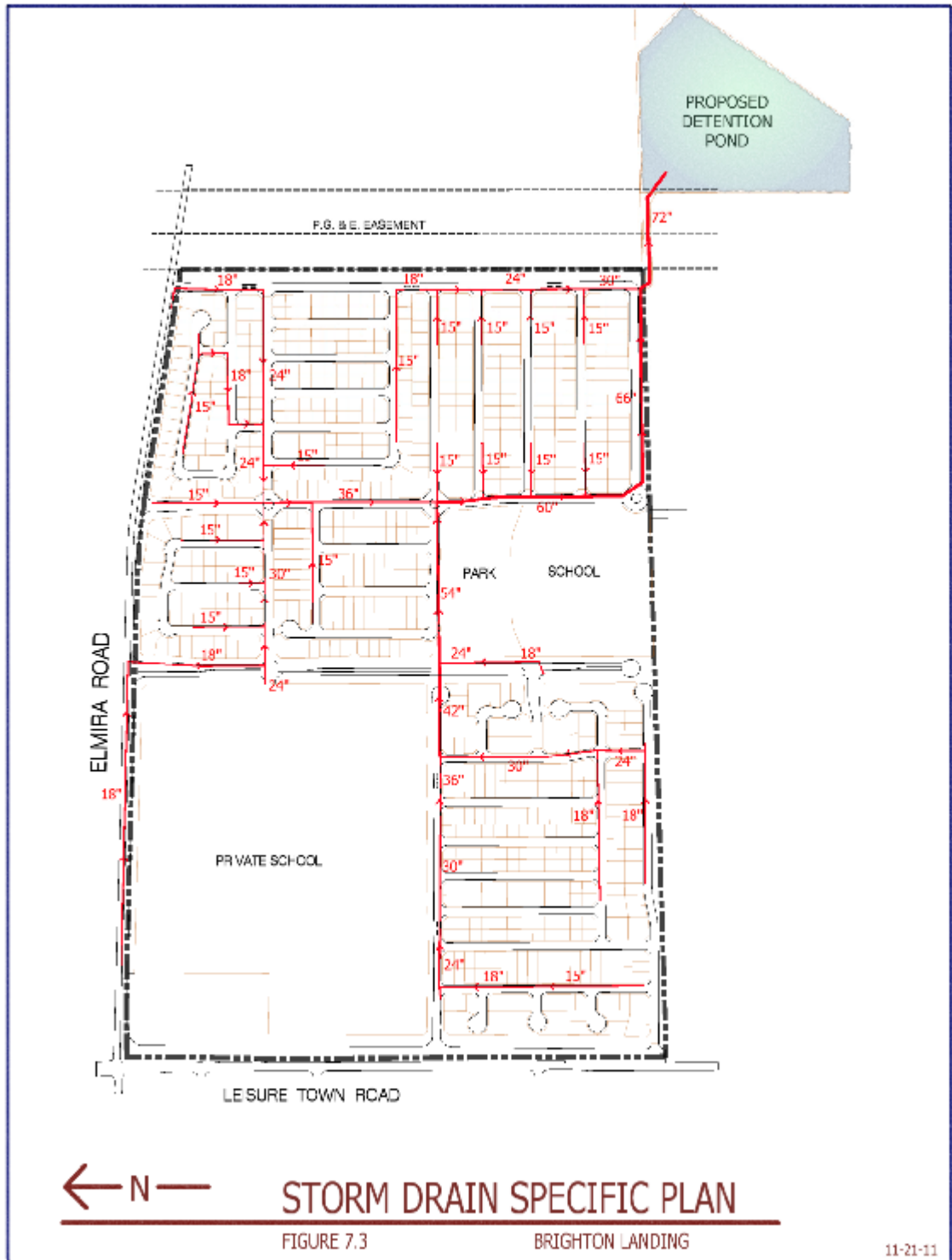
Goal: Provide an adequate, efficient and environmentally compatible storm drainage system for the Brighton Landing Specific Plan area.

7.4.1 Existing Setting

The entire Brighton Landing area is very flat with little grade difference across the property. Generally, the land slopes from west to east with drainage eventually ending up in Alamo Creek. Current City Policy requires that all new development be required to mitigate its drainage through detention such that peak after development run-off does not exceed of pre-development run-off. Because the terrain is so flat, and in order to develop the property without increasing peak run-offs; detention basins will need to be constructed with the use of pumps to discharge the water out of the basins at a prescribed rate of flow. Further, to reduce maintenance costs every effort should be made to minimize the number of basins east of Leisure Town Road. With the development of Brighton Landing, a detention Basin would be constructed on the Batch property adjacent to and east of the PG & E tower lines. This basin would serve a large area (nearly 400 +/- acres if approved with the General Plan Update) and be located in an area that will not interfere with agriculture or future development.

The pipelines through the Brighton Landing development to the detention will range in size from a 72" line discharging into the basin to 15" lines at the upper ends of the system. A layout of the proposed drainage system throughout the Brighton Landing development is shown on Figure 7.3 Storm Drain Specific Plan. Discharge from the basin will be through use of a pump system discharging into a small creek paralleling the railroad tracks which travels north to Alamo Creek.

Funding for the maintenance of the detention basin will be through the formation of a Lighting and Landscape District.



7.5 STORM WATER QUALITY

7.5.1 Introduction

The Brighton Landing project is committed to fulfilling its Clean Water Runoff requirements. As required by the Regional Water Quality Control Board the project will have a Stormwater Pollution Prevention Plan (SWPPP) for the project along with identifying the Best Management Practices (BMPs) planned for implementing and maintaining procedures outlined in the SWPPP.

7.5-P-1 Prepare and implement a comprehensive Stormwater Pollution Prevention Plan (SWPPP) for the project.

7.5-P-2 The Project SWPPP shall identify all Best Management Practices (BMPs) planned for implementation during and following project construction, including Source Control BMPs, Treatment Control BMPs and Post- Construction BMPs. The Brighton Landing Detention Basin is intended to serve as a regional detention basin for both Brighton Landing and the Batch property and as the Post Construction Storm Water Quality BMP device for the area.

7.5-P-3 Require drainage facilities to minimize any increased potential for erosion or flooding.

7.6 FLOODPLAINS

7.6.1 Introduction

The Brighton Landing area is within Zone X (Area determined to be outside the 0.2% annual chance floodplain), as depicted on FEMA's Flood Insurance Rate Map (FIRM) Community Panel Numbers 06095C0281E dated May 4, 2009.

7.6-P-1 Ensure that the project continues to remain outside the floodplain in conformance with FEMA standards.

7.7 NATURAL GAS, ELECTRICITY, TELEPHONE, AND CABLE TELEVISION SERVICE

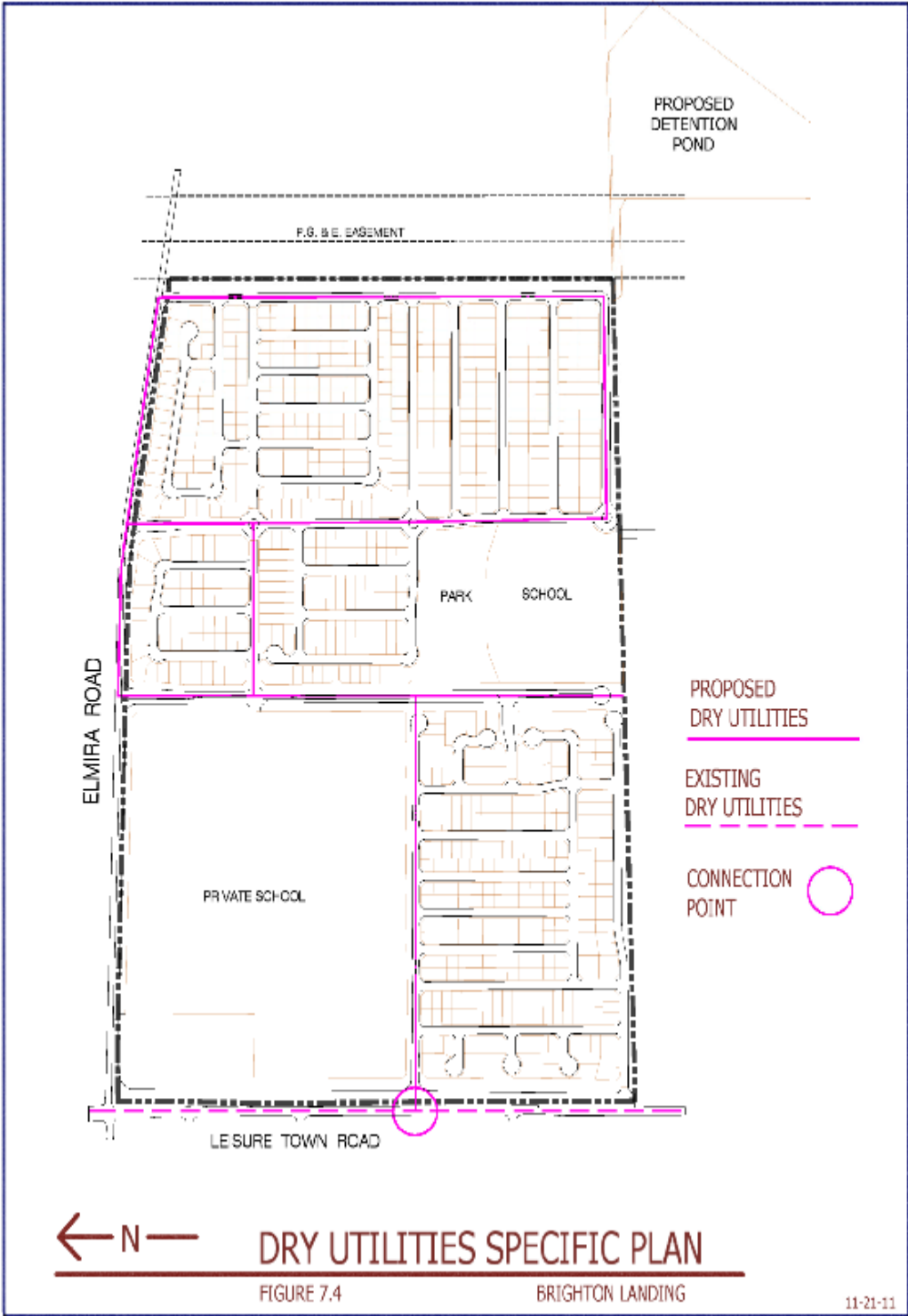
Goal: To provide a full complement of community services as needed in the Brighton Landing area.

7.7.1 Introduction

Gas and electric will be provided by Pacific Gas & Electric (PG&E). Telephone will be provided by SBC, and cable television will be provided by Comcast. All new utilities within the Specific Plan area will be undergrounded. The existing overhead lines within the Specific Plan will be placed underground to the limits of the proposed project.

7.7-P-1 During the improvement plan design process the City of Vacaville and the developer will work with the utility companies to locate their transmission line corridors within the rights-of-way for streets.

7.7-P-2 During the improvement plan design process, the City of Vacaville and the developer will work with the utility companies to ensure that all new utilities and utility vault appurtenances will be placed underground in accordance with the Vacaville Municipal Code.



8.0 COMMUNITY SERVICES AND FACILITIES

8.1 FIRE PROTECTION

Goal: Provide is adequate fire protection in the Brighton Landing Specific Plan area that is consistent with the standards maintained throughout the City.

Currently, fire protection within the Brighton Landing area does not meet current City of Vacaville Fire Department Standards for response times. However, with the construction of a new Fire Station within the Southtown project the Brighton Landing area will meet the response time guidelines. To the extent that the Southtown Fire Station mitigates the need for a fire station in the Brighton Landing area, it is expected that the Brighton Landing development will pay its fair share of fire impact fees to pay for said station. Further, with Brighton Landing's annexation into CFD #12, the station is assured of having adequate staffing.

Brighton Landing is bounded on the north and west by arterial roads which allows for adequate fire protection along these boundaries. However, because of the potential for wild land fires to the south and east these areas are of particular concern. To that end, the Brighton Landing site plan has been designed to facilitate adequate fire protection along the eastern and southern boundaries through the use of irrigated trails and roads.

Also, it should be noted that since all the trails are along streets, proposed fire hydrants meeting City standards for location and fire flow will provide adequate fire protection along the trails.

Lastly, per the adopted City Building Code all houses will have residential fire sprinkler systems.

8.2 POLICE

Goal: Provide adequate police protection in the Brighton Landing Specific Plan area that is consistent with the standards maintained throughout the City.

The Brighton Landing Specific Plan area will be served by the Vacaville Police Department. As with fire protection, the developers of Brighton Landing will pay its fair share of Police Impact fees along with annexing into CFD #12 to ensure adequate staffing for the area.

8.3 PARKS AND RECREATION

Goal: Develop a park and trail system which is designed to meet the needs of the future Brighton Landing Specific Plan area residents.

The proposed park within the Brighton Landing Specific Plan area meets the City of Vacaville requirements for neighborhood parks in a development area of this size. This park, when combined with the elaborate trail system, will provide for numerous outdoor recreational opportunities for the future Brighton Landing residents.

The park and each section of trail will be constructed by the developer responsible for that portion of the project. The costs associated with dedication of the land will be spread among all Brighton Landing developers through establishment of a Benefit District.

8.3-P-1 Prior to the approval of the first final map for the Brighton Landing area, the City and the first developer shall establish funding mechanisms (e.g., Lighting and Landscape Maintenance District and Park Maintenance District, etc.) that provide for the maintenance of the Brighton Landing neighborhood park and trail system. In addition, with the first final map, a Park Fee Agreement will be negotiated which will establish the timing for construction of the Park. It is envisioned that the developer, in lieu of paying park impact fees, would expend certain funds towards the park as development of the project begins (for example land dedication and preparation of the improvement plans). Once a certain number of homes are constructed (to be negotiated) the developer would then begin construction of the park (which may be phased, again subject to negotiation and beyond the scope of this Specific Plan).

8.3-P-2 The park and trail system shall be dedicated to the City of Vacaville with each respective development phase as a public amenity.

8.3-P-3 The Brighton Landing Park should be of high quality and reflect the design theme of the Brighton Landing Specific Plan area.

8.3-P-4 The developers of the Brighton Landing Specific Plan area shall incorporate park and trail facilities to meet the neighborhood park standards specified in the City of Vacaville General Plan for new developments:

8.3-P-4.1 Ensure, as part of the approval process, that each new development provides its share of planned park and trail facilities as shown on Figures 4.3 and 5.1.

8.3-P-4.2 The acreage of these park and trail areas shall meet or exceed the minimum total neighborhood park acreage as specified in the City of Vacaville General Plan for new land use areas.

8.4 SCHOOLS

The Brighton Landing Specific Plan area is within the Vacaville Unified School District boundaries. The District needs to complete a master plan for the east of Leisure Town area to determine the need for new school facilities. The land set aside within the Brighton Landing development will facilitate the construction of a new elementary school. If the District determines that a K-8 school is required (as opposed to a K-6 elementary school) the site set aside in the Brighton Landing area will serve as the northern half of the K-8 school. It is expected that the school size required will be determined over the next two to three years as development occurs within the Brighton Landing area.

It is anticipated that the District will enter into an agreement with the Brighton Landing developer regarding financing of the new school and timing of its construction.

8.5 SOLID WASTE

Goal: Incorporate into all development approvals adequate access for waste haulers and recycling.

Recology has the exclusive right to provide collection and hauling services for solid waste, recyclables, and yard waste as well as street sweeping services in the City of Vacaville through a franchise agreement, subject to limited exceptions stated in the Vacaville Municipal Code.

8.5-P-1 All development within the Brighton Landing Specific Plan shall provide adequate areas for the placement of refuse bins and toters within their respective development areas.

9.0 IMPLEMENTATION

9.1 ADMINISTRATION OF THE SPECIFIC PLAN

The Brighton Landing Specific Plan will be used to direct the processing of future development projects within the planning area. Since there is an extended timeframe for development, and the likelihood that multiple developers will be involved in the development of the Brighton Landing Specific Plan Area, the following responsibilities and procedures will be necessary to review, monitor, coordinate and integrate the stages of development.

9.1.1. Responsibilities for Administration of the Specific Plan

Implementation of the Brighton Landing Specific Plan will be a joint effort between the City of Vacaville and any developer who is proposing to develop in the Specific Plan area or who is a party to the DA with the City.

9.1.2 Development Review

The purpose of this section is to specify the steps required to review new development projects within the Brighton Landing Specific Plan Area. All development projects proposed under this Specific Plan are to be reviewed in accordance with the provisions of the zoning code related to Specific Plans and Policy Plans. Review processes included in this section may be carried out concurrently.

All development within the Brighton Landing area must be consistent with the Specific Plan. Where standards are not contained within the Specific Plan the development must be in compliance with the Vacaville Municipal Code.

9.1.3 Mitigation Monitoring

A program to monitor mitigation measures, adopted as a part of the environmental impact report for this Specific Plan, shall be carried out for all such affected projects in order to ensure compliance with the required mitigation measures during project implementation. An annual review for compliance of the required mitigation measures will be conducted as a part of the Development Agreement Review process.

9.1.4 Project Master Utility Plans and Fair Share Cost Allocation

Given the size of the Brighton Landing Specific Plan and the anticipated time frame for buildout, a Master Utility Plan needs to be filed to detail the required infrastructure to be installed by each developer. As each Tentative Map is submitted, the map will be evaluated based on the Master Utility Plan to specifically determine the improvements required by said map.

With respect to cost allocations for the infrastructure installed, it is expected that a Benefit District will be established by the Master developer and approved by the City Council, which will define the method of cost distribution. Further, if infrastructure items are required and not listed the developer may be eligible for a partial reimbursement from other developers as may be established in a subsequent benefit district.

9.2 PROCEDURE FOR ENVIRONMENTAL REVIEW

The Community Development Director will conduct an environmental assessment on each development application submitted to the City. If subsequent development is in accordance with the Specific Plan and no additional significant impacts can be identified, a new environmental document will not be necessary. If subsequent projects are found to cause additional significant effects, a subsequent EIR or mitigated negative declaration may be required. Subsequent environmental documents will be tiered with the original program environmental document and will incorporate parts of the original document by reference. Section 65457 of the California Government Code indicates that any residential development (including a subdivision or rezoning) that implements and is consistent with a Specific Plan for which an EIR has been certified after 1980, is exempt from additional CEQA review so long as major changes in the project do not occur. Subsequent development applications will be reviewed consistent with Sections 15162 and 15163 of CEQA.



9.3 PROJECT PHASING

While there are four property owners within the Brighton Landing Project area, nearly 97% of the total acreage is under the ownership of Hearthstone and as they develop the Brighton Landing Subdivision, infrastructure improvements will be done by them or their successors. The sole exception to that would be frontage improvements along Elmira Road which is discussed below.

For purposes of project phasing, this Specific Plan will divide the Brighton Landing into four quadrants: The 50 acre northwest quadrant which will be developed by the Catholic High School; the southwest quadrant which is bounded by the High School to the north and the Major Collector Road to the east; and the northeast and southeast quadrants which would each represent one-half of the area east of the Major Collector Road. Figure 9.1 reflects the Master Tentative Map which will create the large parcels, easements, and major road rights-of-way. It is expected that many of the specific details related to phasing will be worked out with Public Works during the Tentative Map entitlement process.

9.3.1 Roads

Elmira Road – Improvements to Elmira Road will be completed by the Catholic High School for their portion of the frontage and the developer(s) of the northeast quadrant for their portion of the frontage.

The small area that fronts the corner parcels would typically be done by the developer, but it is not envisioned that development of the corner will occur in the near future. If development is delayed and completion of the frontage improvements becomes necessary, the City may wish to consider working with the property owner and installing the improvements themselves or working with the Catholic School to install interim improvements to Leisure Town Road (Jepson Parkway) with their project. These interim improvements would include an interim signing and striping plan.

The remaining frontage improvements for Elmira Road will be completed when the property abutting the road develops.

Leisure Town Road (future Jepson Parkway) – Because Leisure Town Road is to be relocated 35 feet to the east, piecemeal construction of the improvements is not practical or feasible. It is envisioned that the total reconstruction will be done by the City of Vacaville. The Brighton Landing developers will be required to pay their fair share of the costs to the city for the frontage improvements for Leisure Town Road (20 feet of payment plus curb, gutter, sidewalk and landscaping). The City in conjunction with the Solano Transportation Authority (STA) will complete the necessary improvements as funding is available. If construction of Jepson Parkway by the City has not commenced by the time development of Phase 12 for Brighton Landing is set to occur, then the developer will install curb, gutter and landscaping improvements along their project frontage. The developer would then fund their fair share portion of pavement in the form of a payment to the City.

Major Collector Road – It is proposed that construction of the Major Collector be delayed until near the end of buildout for Brighton Landing. The major purpose for this Collector Road is to act as parallel reliever route to Jepson Parkway. Until the road can be extended north and south it will serve little purpose. The area east of the Collector will have two connection points to Elmira Road and the area west of the Collector will have two connections to Leisure Town Road. It is therefore proposed that each developer contribute funds (held by the City) such that there are sufficient funds to complete the improvements near the end of the project. As mentioned with other infrastructure improvements, this collector road will be part of the Brighton Landing Benefit District and the amount set aside and the timing of construction will be determined in that document.

Other roads within Brighton Landing - All other roads shown within the Brighton Landing development area will be installed by the developer of that phase where said road occurs. It should be noted that construction of each phase will be required to ensure that there are two ways in and out of any particular village or phase.

Easements required – Since the major area for development is under one ownership, a large lot final map will be processed which will establish the required rights-of-way and ensure that no additional easements are required.

9.3.2 Potable Water

With the development of any phase within the Brighton Landing project area adequate provisions for a “looped water system” must be made in conformance with the Potable Water Specific Plan Layout (See figure 7.1.1). The key components of the water system in the project area comprise the following trunk mains: 12” water trunk line in the Major Collector Road; the existing 12” water trunk line in Leisure Town Road; the proposed 12” water trunk line in Elmira Road and the proposed east-west 12” water trunk line which bisects the area. As each tentative map is processed a water system analysis will be required to ensure adequate water pressure and looping.

Easements required – Again, since the majority of the area of the Specific Plan is under one ownership, a large lot final map will be processed is envisioned which will establish up front the required rights-of-way such that no additional easements are required.

9.3.3 Non Potable Water

As with the Potable Water system, development of any quadrant within the Brighton Landing project area adequate provisions for an appropriate non-potable system must be made in conformance with the Non Potable Water Specific Plan (See figure 7.1.2). The key components of the water system in the project area comprise the pump station and the following trunk mains: 8” water trunk line in a portion of Leisure Town Road; the 8” water trunk line in a portion of Elmira Road; the 8” water trunk line in the Major Collector Road; and the 8” water trunk line in the road at the southern boundary of the Catholic High School.

It is envisioned that the pump station will be installed in later phases of the project buildout. The pump station will be located in the Phase 12 area and it would cost prohibitive to build the pump station early on. The pump

station will be a component of the Brighton Landing Benefit District and the costs to be set aside by each developer and the timing for construction will be established in that document.

With regard to the water line installation, as each phase develops the developer will install the non-potable lines which coincident with their area. Temporary connections will be made to the potable system until construction of the pump station is completed. As with the potable water installation, as each area is developed a water system analysis will be required to ensure adequate water pressure and supply.

Easements required – Again, since all the major area is under one ownership, a large lot final map is envisioned which will establish up front the required rights-of-way such that no additional easements are required.

9.3.4 Sewer

As discussed in Section 7.3 and detailed on Figure 7.2, an off-site 36" sewer trunk main which brings a sewer line to the area will need to be installed with buildout of the Brighton Landing area. However, because it is proposed to develop the project from north to south, it is envisioned that a temporary sewer connection be made to the 54" sewer trunk main in Elmira Road. Up to 385 residential units can sewer into the line meeting the City standards for sewer line construction. As development of the first 385 residential units occurs, the developers will contribute their fair share of the costs of the ultimate 36" line (to be held by the City). The fair share amount and timing of construction of the 36" line will be determined in the Brighton Landing Benefit District.

When the 36" line is installed to the southern boundary of the project area (near Phase 8) the sewer flowing into the 54" line will be diverted into this 36" line by means of a 12" line to be located in the trail area bordering the eastern edge of the project. (See Figure 7.2) Ultimately, this 36" line will be constructed from the existing sewer line near the railroad to the southeast area of the development and will then extent though the project to the Major Collector and eventually to Elmira Road for future extension to the north. As each phase develops they will be required to install the

required sewer system to service their development with their system tying into the aforementioned 36" line.

Easements required – An easement will be required from the Batch Property to make the connection to the existing sewer line near the railroad. Development of the Batch property will require connection to the proposed 36" line and they have indicated a strong willingness to cooperate and have stated that they will grant any required easement. For the balance of the sewer system as mentioned previously, since all the major area is under one ownership, a large lot final map is envisioned which will establish up front the required rights-of-way such that no additional easements are required.

9.3.5 Storm Drain

As detailed in Section 7.4 a detention basin will need to be constructed on the Batch Property at the southeast corner of the Brighton Landing development. The first developer will be required to construct the basin and install the pump station and piping to and from this basin. The developer will be reimbursed through the establishment of a Benefit District.

Subsequent developers of the Brighton Landing area will be required to construct storm drainage lines in conformance with the master plan to insure adequate drainage from their development. To the extent that lines are installed which serve other quadrants, a Benefit District will be established to fairly distribute costs.

Easements required – As with the sewer system, the proposed detention basin will be located on the Batch Property at the southeast corner of the Brighton Landing Project. Since the detention basin is slated to serve both Brighton Landing and the Batch Property the Batch's have again indicated a strong willingness to grant whatever easements may be required.

9.3.6 Neighborhood Park

It is expected that a Park Fee Agreement will be negotiated during the Tentative map entitlement process, by the first developer with the Public

Works Department regarding timing, funding, and possible phasing of the neighborhood park.

9.4 FUNDING MECHANISMS

It is anticipated that construction of the public infrastructure improvements will be constructed by the developers as outlined in Section 9.3 above. It is recognized that some developers may install infrastructure improvements that benefit other developers in the area. A Brighton Landing Benefit District will be established to ensure the fair distribution of costs. We would also note that it is likely that smaller Benefit Districts will be established to ensure the fair distribution of costs between various phases as development occurs.

9.4.1 City Impact Fees

The City of Vacaville has adopted a set of development impact fees to finance citywide capital facilities requirements and sewer, water, storm drain and traffic improvements. The fee structure requires the payment of fees with the building permit and anticipates the construction of capital facilities at the time that development has met an identified impact threshold. Citywide, all new development is required to participate in the program. Fire, police, drainage conveyance (offsite), parks, schools, and green belt buffers are covered by the fee program.

9.4.2 School Impact Fees

The City of Vacaville has enacted a police power ordinance to impose school impact fees on new development. These fees are used to finance the construction of school facilities. It is envisioned that discussions will be held with the Vacaville Unified School District to agree upon the method of financing the public school within the Brighton Landing development area.

9.4.3 Park and Recreation Fees

The City of Vacaville has adopted a police power ordinance to impose fees for parks and recreational uses on new development. The developer of the quadrant that includes the park would construct the park and negotiate a park fee agreement with the City of Vacaville to obtain credits for the construction of park improvements. The costs associated with dedication

of the land would be spread among all the Brighton Landing developers through a Benefit District.

9.4.5 Brighton Landing Benefit District

The Brighton Landing project will be installing improvements that benefit properties to the north and south of Brighton Landing. As other developments tie into those facilities they will reimburse the Brighton Landing developers based on a Benefit District that will be established and approved by the City Council for the City of Vacaville. The Benefit District insures that future developers reimburse their fair share of improvements installed that benefit them.

9.5 MAINTENANCE

9.5.1 Public Safety District (CFD #12)

Public Safety District also known as Community Facilities District (CFD) #12 has been established by the City Council to pay for police and fire services in new development areas within the City. Brighton Landing will be required to annex into CFD #12.

9.5.2 Lighting and Landscape District

The Brighton Landing area will be required that a Lighting and Landscape District be formed for ongoing maintenance of the backup landscaping, trail landscaping, and street lights in the area.

9.5.3 Parks Maintenance District

The Brighton Landing area will be required that a Park Maintenance District be formed for ongoing maintenance of the park in the Brighton Landing area.

9.6 PROCEDURE FOR AMENDING THE SPECIFIC PLAN

The procedure for amending the Specific Plan as specified in the Land Use and Development Code (Chapter 14.09.112) requires submittal of an application and accompanying processing fees. Before amending the Specific Plan, the City will refer the action to the appropriate agencies that will participate in the review of the proposed amendment. The City will also provide opportunities for the public to become involved in any future

amendment. Amendments are permitted to the Specific Plan, without the necessity to amend the Development Agreement, as long as such amendments are not inconsistent with the Development Agreement. Amendments may be initiated by a property owner or by the City.

9.6.1 Amendment with Rezoning

If the Community Development Director determines that a zone change is required, said applications shall be reviewed in addition to the Specific Plan Amendment itself, and may be reviewed concurrently.

9.6.2 Administrative Changes

Given that the buildout period for the Specific Plan will occur over several years and that the figures are illustrative, the Community Development Director may make administrative changes to the Specific Plan. These administrative changes are not considered amendments:

- a) Changes to the provision of community infrastructure, such as drainage, water and sewer systems that do not have the effect of increasing or decreasing the development capacity in the Brighton Landing area.
- b) Changes to Land Use or Phasing Area boundaries that would not affect dwelling unit ceiling or infrastructure sizing.

9.7 ADMINISTRATION OF THE DEVELOPMENT AGREEMENT

Annually on the anniversary of the signing of the Development Agreement, the developers for Brighton Landing will submit a report on the status of the agreement as provided by the Vacaville Municipal Code. The report will include the status of measures assigned to the developer for implementation by the Development Agreement and the Mitigation Monitoring Program.

An amendment to the Development Agreement necessitates the concurrence of the landowners and the majority of the City Council. Amendments will require a duly noticed public hearing.